

FINDRASSIE¹

MASTERPLAN / A NEW NEIGHBOURHOOD FOR ELGIN

SUPPLEMENTARY GUIDANCE VOLUME 3: BACKGROUND INFORMATION

THE MASTERPLAN PROCESS
CONTEXTUAL ANALYSIS
ENGINEERING
TRANSPORT
FUTURE APPLICATION CHECKLIST

7.0 : THE MASTERPLANNING PROCESS



7.1 : INTRODUCTION

Findrassie will be different. Findrassie will bring a new approach to planned development in Elgin.

FINDRASSIE IS A PIONEERING PROJECT FOR MORAY COUNCIL IN CREATING A HIGH QUALITY NEW DEVELOPMENT IN ELGIN

The Moray Council and the land owner are fully committed to creating a quality new residential and employment areas in Moray which meets national and local planning objectives. The Council has put Placemaking as one of its top three, overarching policies in the Local Development Plan. A Design Champion has been appointed to focus the attention of all parties on increasing the quality of design. Extensive and collaborative training programmes in design have been rolled out by the Council to its own staff, elected members, community representatives and the wider development community of Moray. The Council works with Architecture and Design Scotland to integrate a Design Review process in strategic masterplans.

FINDRASSIE IS A COLLABORATIVE MASTERPLAN

The Findrassie Masterplan has been developed as collaboration between Pitgaveny, the Council, the community and key stakeholders including Scottish Natural Heritage, SEPA, RSPB and Architecture & Design Scotland with the aim of creating a higher standard for Placemaking in the area.

FINDRASSIE IS A LONG TERM DEVELOPMENT

Pitgaveny, the owners of the land at Findrassie and the adjacent land including Findrassie woods, have a long term commitment to the success of Findrassie. They are committed to finding the best developer partners to deliver the housing and other uses. They plan to stay involved in the development in the long term which could include design review of new detailed proposals, retaining ownership and undertaking a management role in its open spaces to retain the distinctiveness and unity of these vital spaces over time as the development progresses. Pitgaveny want their land to be a legacy of the best that can be achieved in planned development. 1,500 new houses will be a 15% increase to Elgin's 10,000 dwellings. It will take about 25 years to build out. So the Masterplan and the Partnership of Landowner, Council, and Community will play a critical role in keeping a high quality of development over a long period of time.







7.1 : INTRODUCTION - CONTINUED

This will ensure that as the development grows in phases, the trees and landscape areas will grow with it; and as the population increases, so will the infrastructure. New and improved roads, cycle tracks, footpaths, green networks and spaces, a community “hub”, a commercial area, and a new primary school will be built in step with the progress of the new neighbourhood.

FINDRASSIE WILL NOT SIMPLY BE A ROLL- OUT OF NEW DEVELOPMENT;

Although this is a large open “greenfield” site of over 100 hectares, stretching northwards from the built-up edge of Elgin, Findrassie will enhance Elgin as a whole, as it will provide a choice of location and type of environment in which to live.

ARCHITECTURE + DESIGN SCOTLAND

A+DS have been involved in the development of the masterplan at key stages of the design process. The design team has been involved in two workshops which have established and developed the principles of the new masterplan and the vision for Findrassie. A third workshop is programmed to take place in October this year and will further inform the proposals. The involvement of A+DS has been helpful to the design team and should give the local authority the confidence that the supplementary guidance provides a strong and robust framework which will inform future planning applications and ensure that the vision is delivered.

DESIGN REVIEW

There have been a number of design reviews of the emerging proposals, not only by A+DS but by the local authority and internal stakeholders and by the general community as a whole. Most recently and notably these have included:

Council stakeholder meeting - February 2015

Council stakeholder workshop - May 2015

Community workshop including local councillors / community councils - May 2015

Community Exhibition - May 2015

Pitgaveny Farm Open Day and Youth Workshops - May 2015

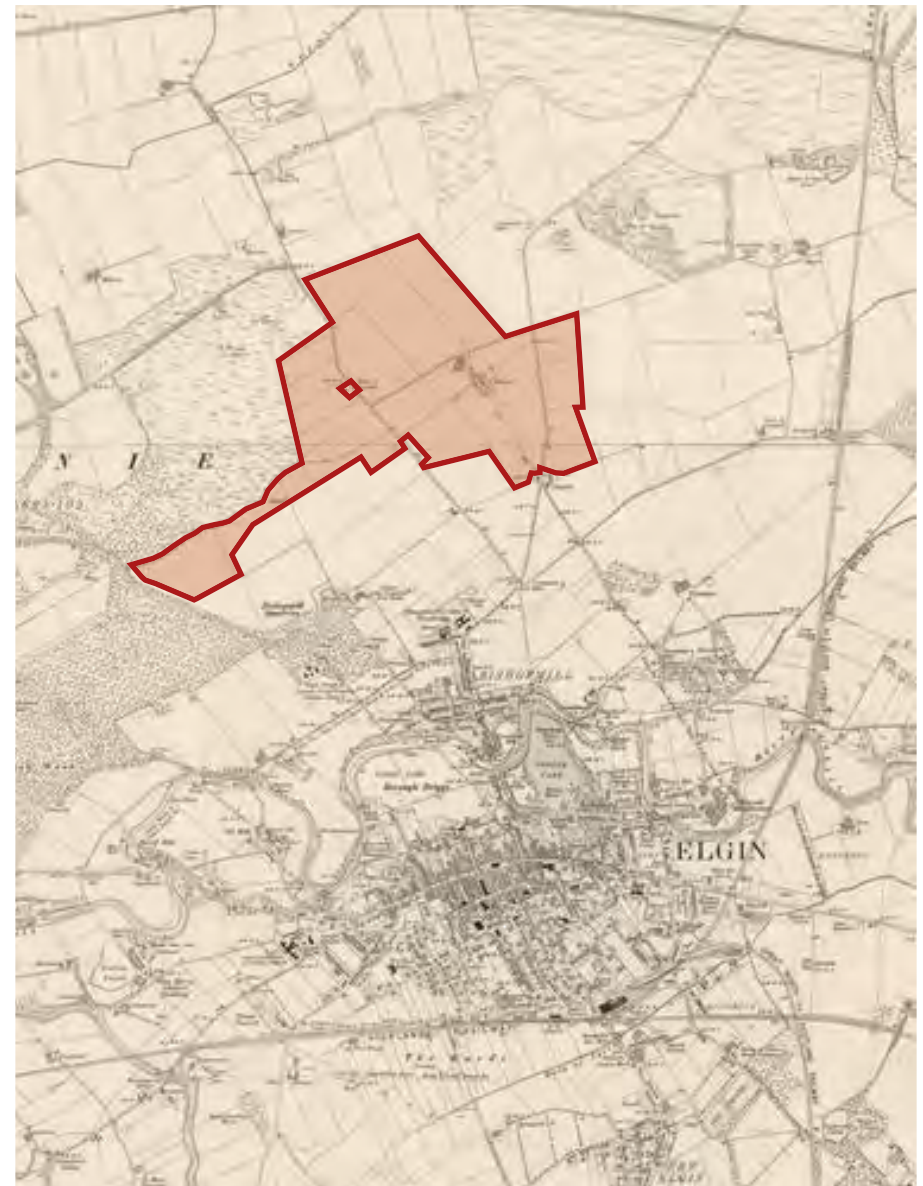
Presentation of the emerging proposals to council / external stakeholders - June 2015

Internal workshops - June 2015

7.2: PLANNING CONTEXT



EXTRACT FROM ROY MILITARY SURVEY (1747-52)



EXTRACT FROM ORDNANCE SURVEY (1868-70)

7.2.1 : PLANNING HISTORY



EXTRACT FROM ORDNANCE SURVEY (2015)

In order to achieve the desired step-change in the quality of new residential and employment areas in Moray to meet national and local planning objectives, the Findrassie Masterplan has been developed as collaboration between the landowner, the Council, the community, and key stakeholders with the aim of creating a new standard for Placemaking in the area. This approach has been made more possible by the long term identification of the site as a development area.













Findrassie is allocated in the Moray Local Development Plan 2015. It covers the allocations of:

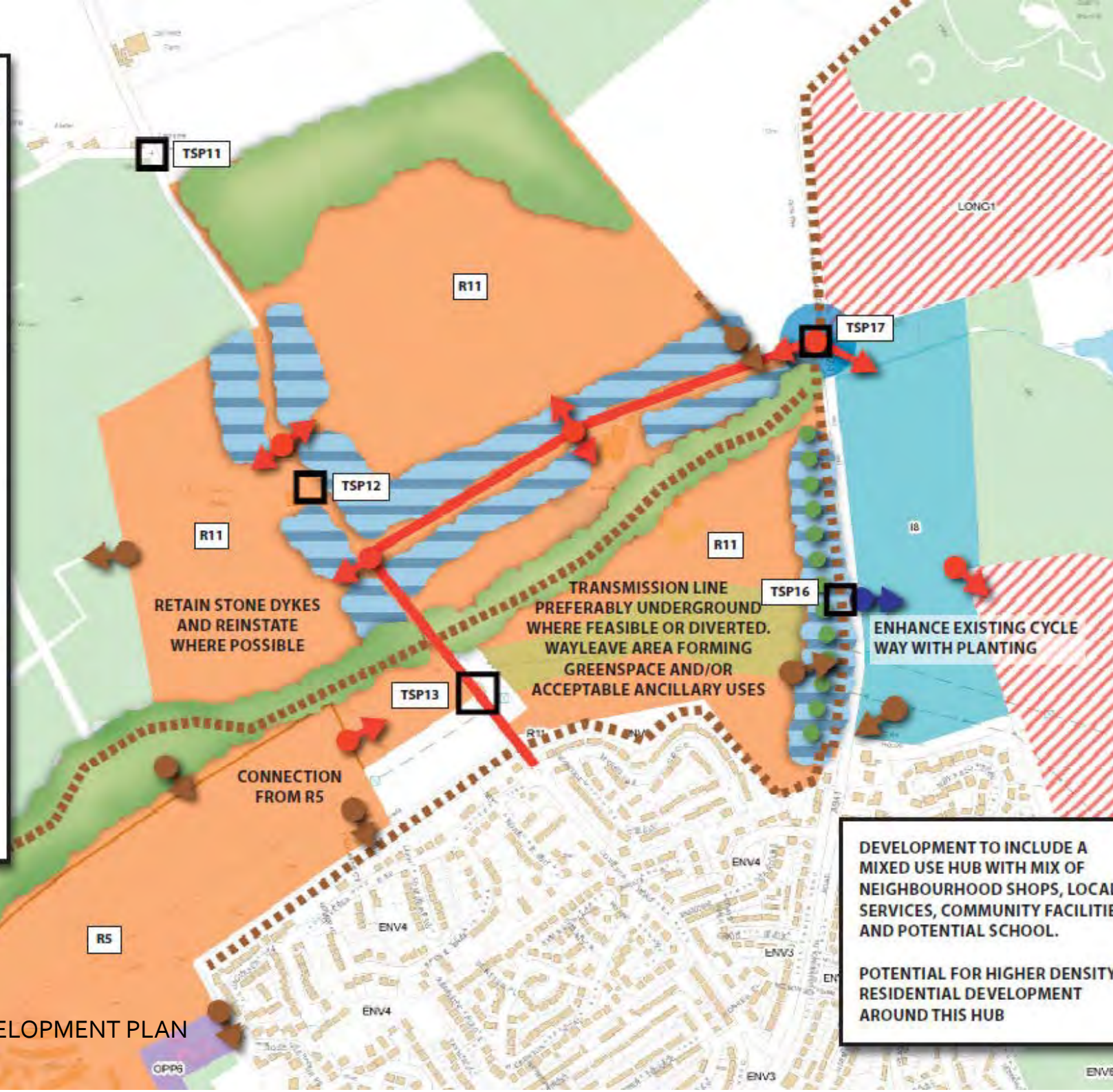
- R11 Findrassie/Myreside which covers 100 hectares with an indicative capacity for 1500 houses and;
- I8 Newfield which covers 12 hectares of land considered suitable for Class 4 business uses.

The site has been in the pipeline for development for some time. Findrassie was allocated as a "LONG" site in the Moray Local Plan 2008 in order to indicate the general, longer term direction for housing development. The Moray Council uses the designation LONG in order to provide a strategic view of the future direction of growth for some settlements and to ensure that an effective land supply is provided. The significant advantages of this approach in the case of the planned new neighbourhood at Findrassie are that:

- The population of Moray, and particularly the residents of Elgin, have known for a considerable time that these fields are highly likely to be developed and have had time to adjust to these proposed changes;
- It has been possible to plan ahead for the loss of farmland to the wider Pitgaveny Farms business and to restructure parts of the farming operation to accommodate this;
- It has allowed long-term, strategic masterplanning to be undertaken

FINDRASSIE/MYRESIDE KEY DESIGN PRINCIPLES

-  Green corridor with recreational path
-  New woodland structure planting - Planting should aim to reduce visual prominence and a mix of native species should be planted. Some evergreen species should be included within the mix to provide year round cover (e.g. holly, juniper, Scots Pine)
-  Buildings should front onto main vehicle routes
-  Access all modes
-  Existing road upgraded to form primary spine route
-  Secondary Access
-  Sense of arrival-gateway feature or building
-  Feature trees and planting along existing cycleway
-  Open Space
-  Existing Cycle/foot path
-  Footpath/cycle link connections
-  Key transport improvements (TSP) (see schedule)



EXTRACT MORAY LOCAL DEVELOPMENT PLAN

DEVELOPMENT TO INCLUDE A MIXED USE HUB WITH MIX OF NEIGHBOURHOOD SHOPS, LOCAL SERVICES, COMMUNITY FACILITIES AND POTENTIAL SCHOOL.

POTENTIAL FOR HIGHER DENSITY RESIDENTIAL DEVELOPMENT AROUND THIS HUB

7.2.1 : PLANNING HISTORY - CONTINUED

- It has provided a reserve of land which was able to be brought forward during the life of the Local Plan when shortages in the five year land supply were identified. An initial 350 housing units at Findrassie were allocated in June 2013 by the identification of a shortfall and has now been confirmed as an allocation in the Moray Local Development Plan 2015

Elgin is the “Primary Centre” in the LDP Spatial Strategy therefore the majority of new development is to be directed here to support the Moray Economic Strategy and enhance its role as a regional centre. The northern growth of Elgin at Findrassie is the major expansion area for the town over the next decades and will provide land for most of Elgin’s housing and employment needs in that time scale.

In order to achieve the significant improvements in the quality of new residential and employment areas in Moray, The Moray Council has set three Primary Policies in the LDP of: PP1: Sustainable Economic Growth; PP2: Climate Change; PP3: Placemaking. The allocation of Findrassie and the Masterplan meet the objectives of each of these policies by:

PP1: Sustainable Economic Growth: Providing a 12 hectare employment land designation in a sustainable location, which is connected to Elgin and Lossiemouth by active travel routes and public transport. Findrassie was chosen because of its sustainable location on the edge of Elgin within 20 minutes’ walk/10 minutes’ cycle of the town centre and on a cycle route to Lossiemouth.

PP2: Climate Change: A detailed flood risk assessment has been undertaken and a sustainable drainage strategy developed. The masterplan will require planning applications to provide details of their approach to sustainable construction and energy efficiency and wherever possible, the scope to install low and zero carbon generating technologies.

PP3: Placemaking: Both the process of developing the Findrassie Masterplan and the Masterplan itself, have emphasised the key principles of Placemaking. A design-led, approach responding to the context and character of the site are the hallmarks of this exercise. Full involvement of the community and other key stakeholders has anchored the designs in the local environment and culture. Strong themes of north East traditional street and building line patterns have influenced the distinctive grid layout of the masterplan. The Masterplan document sets out how the key Placemaking requirements have been met in the designs.

The key Local Development Plan policies relating to Findrassie are listed in a table within the Appendix.

Planning applications for the various elements of Findrassie must take these policies into account.

The Moray Council has also produced Supplementary Guidance on Urban Design, to which this masterplan adhered (referred to in detail in section 5).

The Moray Local Development Plan 2015 contains a set of design principles which have guided the Masterplan’s development.

7.2.2: POLICY

PP3 Placemaking

All residential and commercial (business, industrial and retail) developments must incorporate the key principles of Designing Streets, Creating Places and the Council's supplementary guidance on Urban Design.

Developments should;

- create places with character, identity and a sense of arrival
- create safe and pleasant places, which have been designed to reduce the fear of crime and anti social behaviour
- be well connected, walkable neighbourhoods which are easy to move around and designed to encourage social interaction and healthier lifestyles
- include buildings and open spaces of high standards of design which incorporate sustainable design and construction principles
- have streets which are designed to consider pedestrians first and motor vehicles last and minimise the visual impact of parked cars on the street scene.
- ensure buildings front onto streets with public fronts and private backs and have clearly defined public and private space
- maintain and enhance the natural landscape features and distinctive character of the area and provide new green spaces which connect to green and blue networks and promote biodiversity
- The Council will work with developers and local communities to prepare masterplans, key design principles and other site specific planning guidance as indicated in the settlement designations.

Justification:

The Scottish Government aims to encourage higher standards of urban design and has published Designing Streets and Creating Places. The Council has approved supplementary guidance on urban design which developers are referred to.

The planning system encourages a designed approach to planning responding to the context and characteristics of each site. The 6 key qualities of distinctive, welcoming, adaptable, resource efficient, safe and pleasant and easy to move around and beyond should be considered and integrated into new developments.

Design statements are required for all major applications, however this approach is encouraged for all developments over 10 units to understand the proposal within the context of the site and surrounding environment.

Scottish Government policies encourage the creation of walkable neighbourhoods which are defined as neighbourhoods which have a range of facilities within 5 minutes (about 400 metres) walking distance of residential areas.

A Design and Access Statement is required for national and major planning applications and the Council encourages preparation of a Design Statement for all housing developments of 10 or more units and commercial developments in excess of 500 sq. metres.



7.2.2 : POLICY

The Masterplan process has been assiduous in seeking to meet the key aspirations of National Planning Framework 3 and Scottish Planning Policy to make Findrassie:

- A successful, sustainable place
- A low carbon place
- A natural, resilient place
- A connected place

The “Design-led” approach and the six qualities of successful place as advocated by SPP and contained in Designing Streets, Creating Places have dictated the framework produced. Later sections of this document illustrate how Findrassie is to be:

- Distinctive
- Safe and Pleasant
- Welcoming
- Adaptable
- Resource Efficient
- Easy to Move Around and Beyond

The NPF3 highlights Elgin as “A diversifying town: The Elgin ‘City for the Future’ study noted the town’s important role as a regional leader, providing a good range of services. Its vision is to stimulate business development, diversification and innovation.”

The other key National Policy documents relating to the Findrassie Masterplan are “Designing Streets” and “Creating Places”. The Council’s Placemaking policy PP3 distils the key features of these documents. The Findrassie Masterplan meets these policy objectives as set out later in this document.

7.2.3 : MORAY LOCAL DEVELOPMENT PLAN 2015

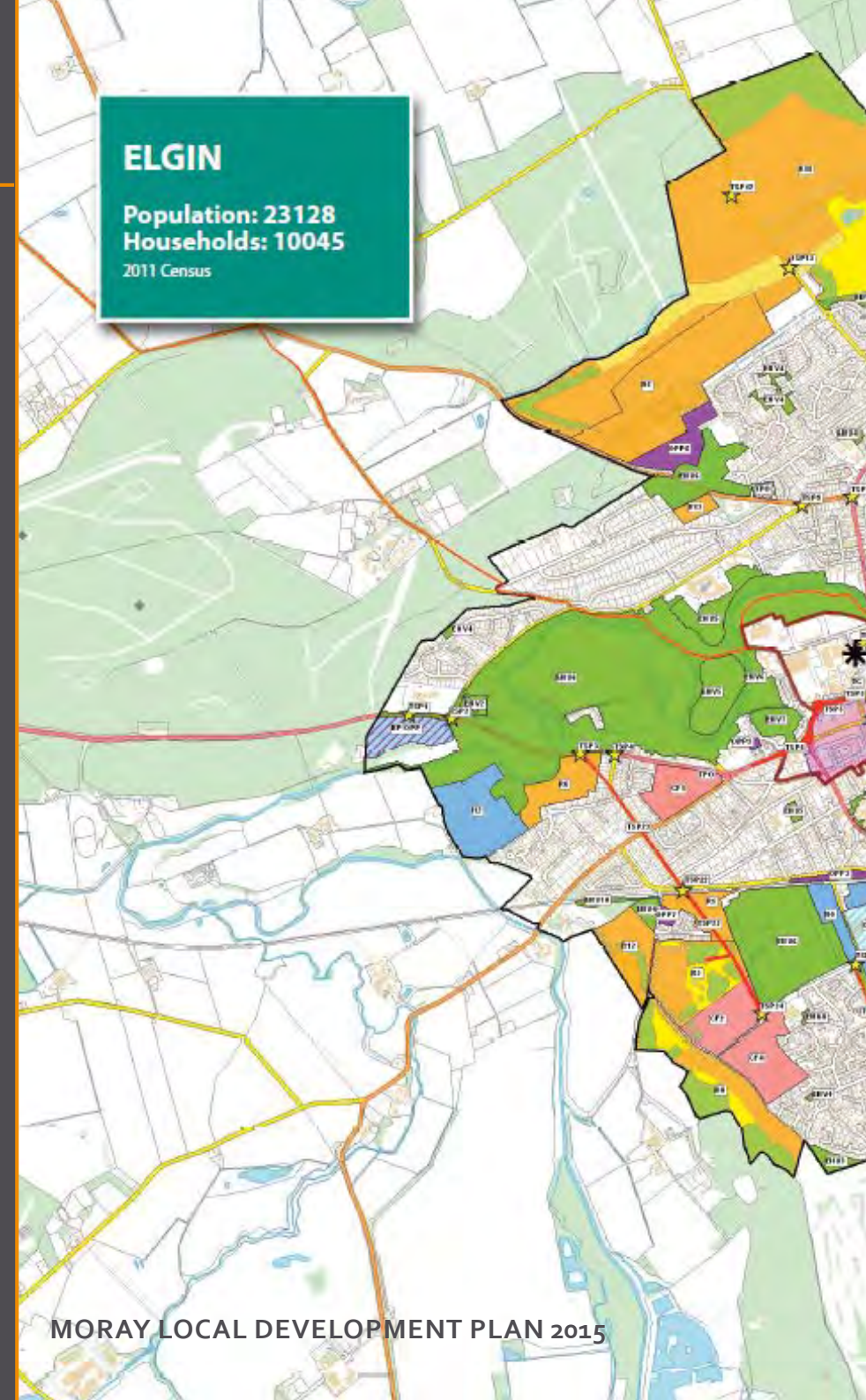
R11 Findrassie/Myreside Site :100 ha Indicative capacity for 1500 houses

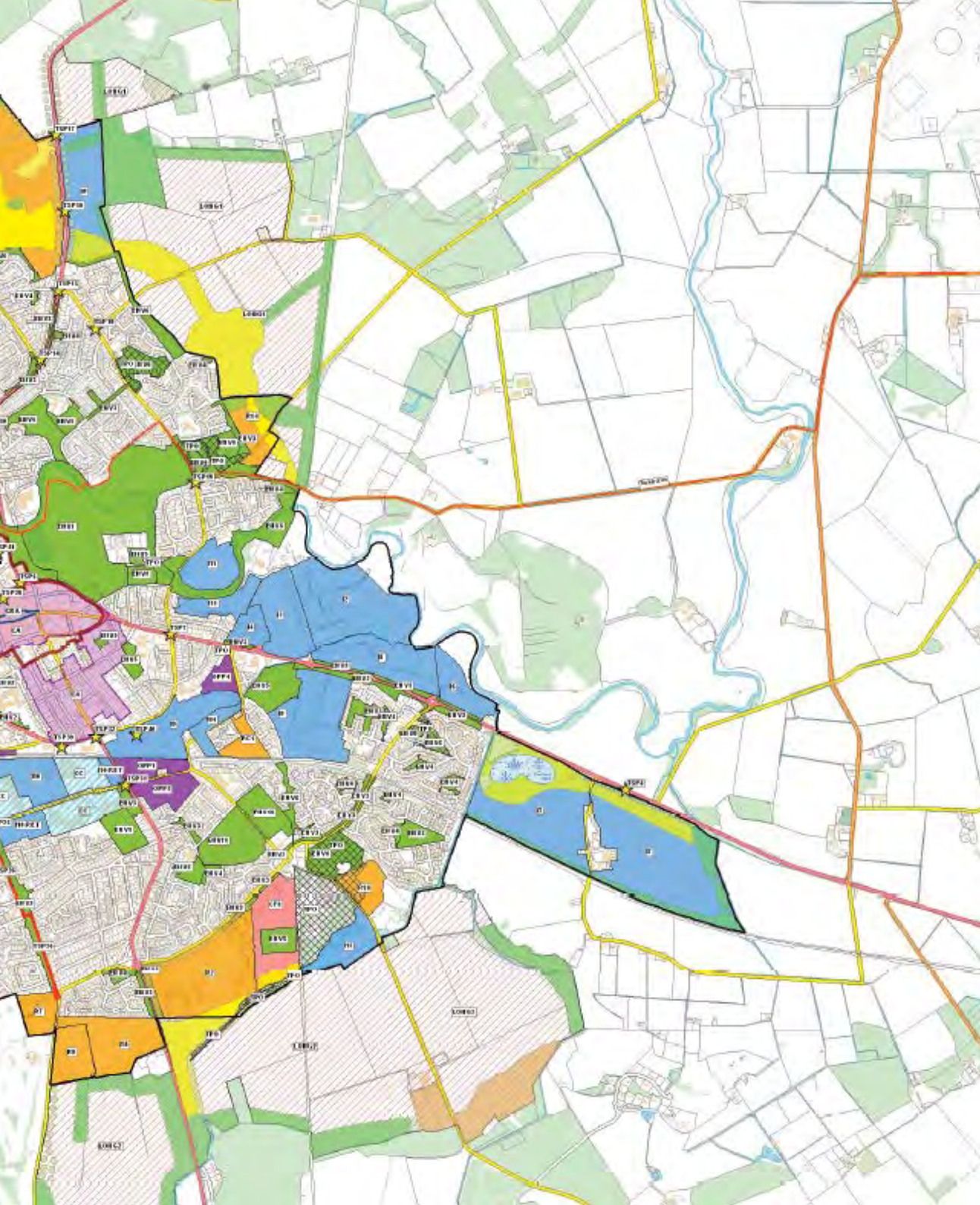
Previously identified as a LONG site, this is now brought forward as the major new land release for Elgin. The site extends to approximately 100 hectares and has capacity for 1500 houses.

A masterplan must be prepared and the development phased to create a distinct identity. The design principles set out in the accompanying map should address the key design principles set out in the accompanying map. Access to I8 should be considered in conjunction with this site. Development proposals will require to safeguard the integrity of Loch Spynie SPA. Adequate SUDs provision should be made. Flood risk may constrain parts of the site and a flood risk assessment will require to accompany proposals for development. Water resilient measures should be considered as part of this. A buffer strip of at least 6 metres between the development and the watercourse is required.

A Transport Assessment must be submitted with proposals and early contact with Moray Council Transportation is essential. Off site junction improvements will be required. Connection should be made with access arrangements for R7 and a new/upgraded junction onto the A941 is required. Widening and improvements required to Covesea Road and Myreside Road. Footway, cycleway and public transportation connections required. The impact on the junction TSP31 must be considered and a contribution to any necessary mitigation addressed.

A buffer strip of at least 10 meters between the development and the watercourse is to be provided. A Phase 2 Habitat Survey shall be provided in support of all future planning applications.





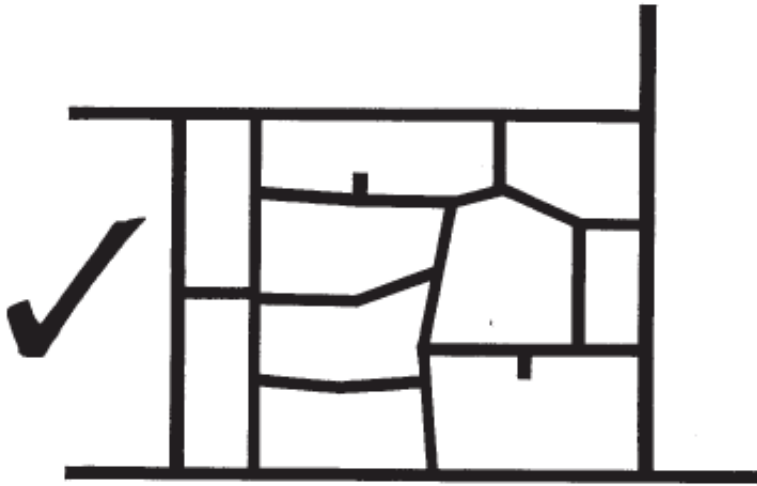
18 Newfield (12 hectares available)

"This site is suitable for business uses within use Class 4 (business). Development of offices, research and development facilities and light industrial uses in class 4 will be supported. The site should have a high amenity setting and a landscape and planting scheme must be submitted with proposals. This must consider the context of the site and future development at LONG 1. An access strategy for this site should be considered as part of the masterplan for R11 and future connections to LONG 1. A Transport Assessment will be required and the following junctions must be considered TSP 16 and TSP 17.

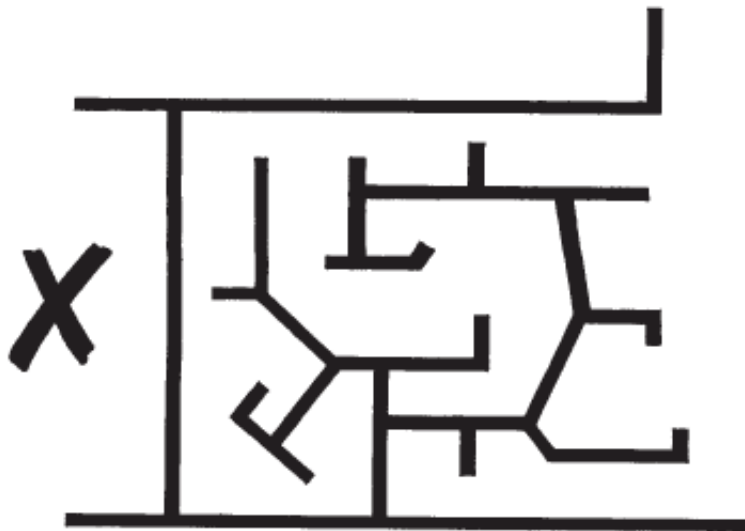
A walkover and photographic survey of habitats is required to assess the presence of wetlands and to identify any consequent requirement to address/mitigate the impact on groundwater dependent terrestrial ecosystems. Development proposals will require to safeguard the integrity of Loch Spynie SPA. Adequate SUDS provision should be made."

A Phase 1 Habitat Survey shall be provided in support of all future planning applications.

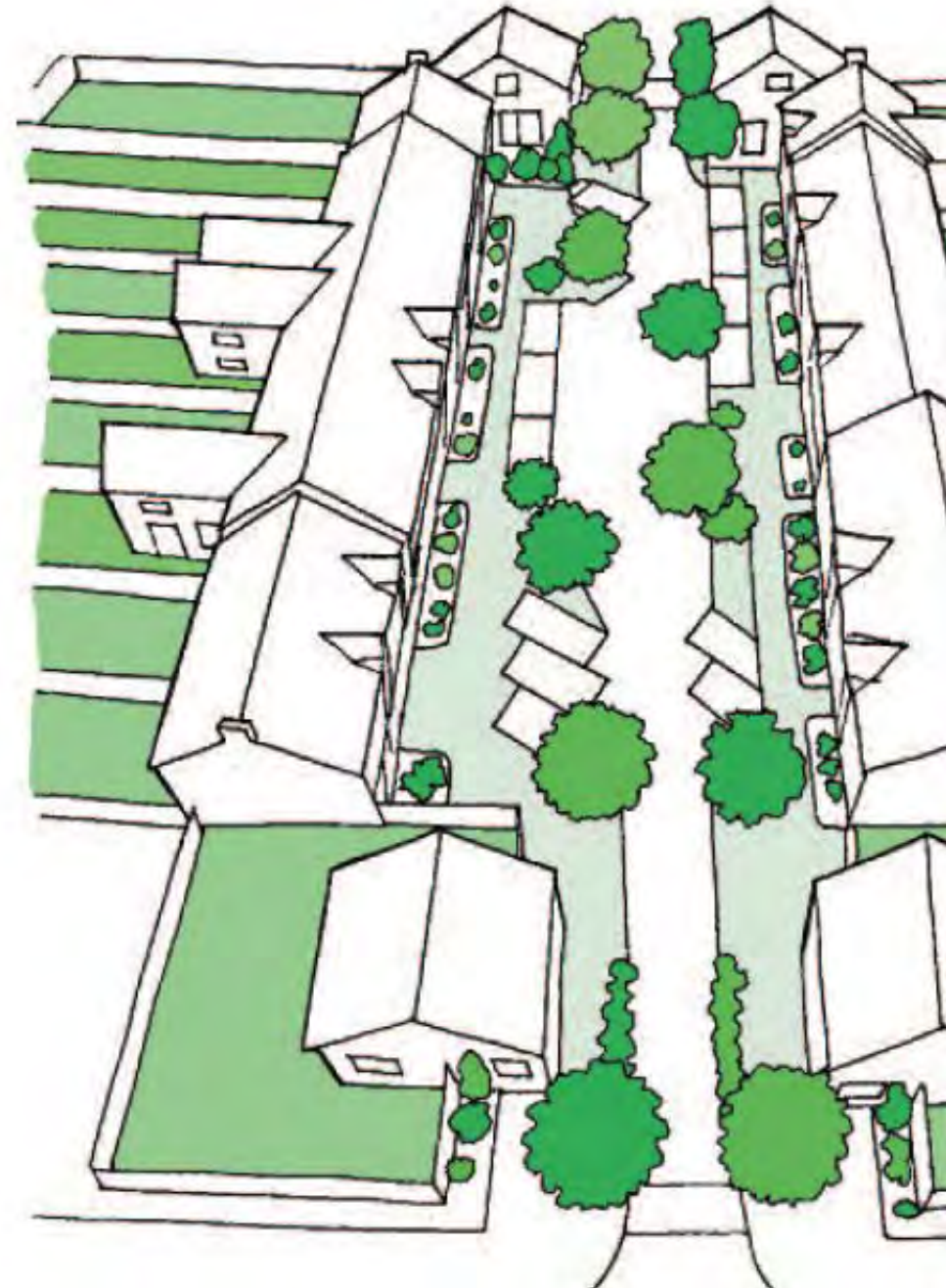
7.3 : SUPPLEMENTARY GUIDANCE INTRODUCTION



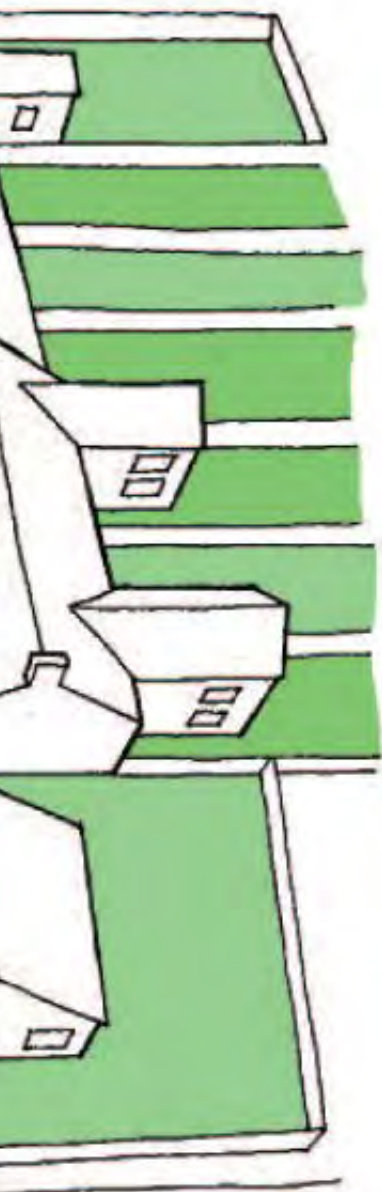
A well connected permeable street pattern



A poorly connected street pattern dominated by cul de sacs



HOUSING STREETS



Extracts from The Moray Council's
Supplementary Guidance on Urban Design

7.3 : SUPPLEMENTARY GUIDANCE INTRODUCTION

The Scottish Government introduced the concept of statutory supplementary guidance, which would form part of the development plan. The purpose of Supplementary Guidance is to shorten development plans and make it easier to keep them up to date and to provide more detailed guidance on specific issues arising from the Development Plan. Typically Supplementary Guidance covers development briefs or master plans which provide a detailed explanation of how the council would like to see particular sites develop, or strategies or frameworks on specific issues and/or detailed policies.

The Findrassie Masterplan is Supplementary Guidance approved by The Moray Council having been subject to full public consultation procedures. As Findrassie is the major new land release for housing and employment land in Elgin, the Moray Local Development Plan 2015 requires a masterplan to be prepared for this area. The role of the masterplan will be to provide a robust framework to guide all parties involved in the implementation of Findrassie.

As such the Findrassie Masterplan Supplementary Guidance must be treated as a material consideration in the development management process and given significant weight in the consideration of all planning applications coming forward for the site.

Proposals which are considered to be departures from the provisions of the Findrassie Masterplan will be treated as departures from The Moray Local Development Plan 2015 . The Masterplan fulfils the role of Supplementary Guidance which has been approved by the Planning Authority.

Supplementary Guidance can in certain instances be subject to Strategic Environmental Assessment (SEA) and Habitats Regulations Assessment. The Moray Council is the Responsible Authority in this instance and therefore undertook a screening request to the SEA Gateway. The result was received on 27 April 2014 and the Consultation Authorities (Historic Scotland, Scottish Environment Protection Agency and Scottish Natural Heritage) individual views were that there was no likelihood of significant environmental effects. Given the LDP has been subject to an extensive SEA and HRA and that there is no likelihood of significant effects associated with development of the masterplan area, the Planning Authority as the 'responsible authority' has taken the view that these assessments are not needed.

7.4: COMMUNITY ENGAGEMENT

Involving as wide a constituency of people as possible in the development of the Masterplan was a key element which has added considerable value to the Findrassie project and will be a part of future stages of the implementation of the new neighbourhood.

Stakeholders involved;

- The Moray Council's Internal/Other Stakeholders Group Various workshops and interactions throughout 2014 and 2015
- The Wider Community (Workshops and Exhibition on 22 and 23 April)
- Community Councils
- Elected Members
- Architecture and Design Scotland
- The Client Team
- Tenants residents and employees of Pitgaveny
- The Planning Policy Team
- Direct with stakeholders over discipline-specific matters.
- The Elgin Youth Café
- The Pitgaveny Farms Open Day
- The Pitgaveny Farms Education Day
- A specific response by the Elgin Community Council
- Ongoing informal interactions with Council and other counterparts on matters of detail.

Consultation was tailored to suit each group best:

Workshops were held with the Council and Stakeholders group, with Architecture and Design Scotland, the Community Councils and Elected members.



FINDRASSIE

HELP US PLAN A NEW NEIGHBOURHOOD FOR ELGIN

Elgin is growing and a new neighbourhood is being planned to the north of the town. Findrassie will provide Elgin with new, attractive, affordable homes and offer people space to work, learn and relax. We want to hear your ideas so we can create a well-planned community that Elgin can be proud of.



CONSULTATION EVENT - WEDNESDAY 22ND APRIL 2015 - St Giles Church

6PM-8PM - WORKSHOP

(MEMBERS OF THE LOCAL COMMUNITY ARE INVITED TO BOOK A PLACE BY CONTACTING:

Craig Main of EMA Architects + Masterplanners 0131 2471450 / craig.m@ema-architects.co.uk)

COMMUNITY EXHIBITION - THURSDAY 23RD APRIL 2015 - St Giles Church

OPEN FROM 1PM-8PM - PUBLIC EXHIBITION AND WORKSHOP

(MEMBERS OF THE LOCAL COMMUNITY ARE INVITED TO DROP IN TO THE EXHIBITION TO LEARN MORE)

COME ALONG AND HAVE YOUR SAY

ALTERNATIVELY VISIT OUR WEBSITE AT: www.findrassie.co.uk

FURTHER ENQUIRIES: info@ema-architects.co.uk



FINDRASSIE - 2015

WHAT HAPPENS NEXT?

PITGAVENY



WHAT HAPPENS NEXT?

Subject to this consultation exercise and further discussions with the local community and local authority, it is proposed to submit the Supplementary Guidance for inclusion within the Moray Local Development Plan later this year.

The design team will take account of all comments made during this process as they further develop the proposals for the new neighbourhood.



OPPORTUNITY FOR INVOLVEMENT:

We welcome your comments on what you have seen and would be grateful if you could fill in the comments forms provided.

Alternatively, if you wish to think a little bit more about the development then return the questionnaire to us by post or by email. Contact details are provided at the bottom of the comments forms.

You can also view the proposals on the website:
www.findrassie.co.uk



ema

Lead/Author:
Planning Consultant
Architect
Engineering Consultant
Landscape Architect
Transport Consultant

Project:
Safely Gate
Safely Gate
Safely Gate
Safely Gate
Safely Gate
Safely Gate

7.4: COMMUNITY ENGAGEMENT



Findrassie feedback 'remarkably positive'

PLANS to build up to 1,500 new homes to the north of Elgin have received a positive response, according to the team behind the project.

A series of public consultations were held last week at St Giles' Church in Elgin to gauge opinion on the 'Findrassie' scheme, which would be spread over 276 acres and include businesses and parks.

A new primary school may also be included, in a development that would be built over the next 20 to 25 years.

The Findrassie project forms part of Moray Council's development plan, and will be built on land owned by the Pitgaveny estate.

It would be built in up to five phases.

Donald Lunan, a spokesman for Smiths Gore, agents for Pitgaveny, described the feedback from the sessions as "remarkably positive".

"Most people, if there were any concerns, said it was about the scale of the development, and that is why there needs to be a reassurance that it is a 20- or 30-year plan and a very long timescale," he said.

"One of the comments I heard was that 'This is a huge development for Elgin, but another person said 'But at least it's being properly planned.' That is what we are trying to ensure that we do.

By JOE MILLICAN

j.millican@northern-scot.co.uk

"It was good engagement all round, and hopefully the next time we will be that much further on with the planning and design."

Pitgaveny is currently working closely with the local authority to agree a master plan to determine how the area might be designed.

This master plan would then control how individual parts of the neighbourhood are built, accessed and landscaped, before the first planning application – perhaps as early as 2016 – is considered. Any future developer would then be expected to adhere to the plan.

The proposals will now be presented to Moray Council for their consideration.

No developers are yet involved in the scheme, and if the proposals for the master plan are approved by Moray Council, they would have to bid for individual parcels of land.

Much of the Findrassie project would sit immediately to the north of a development by Robertsons for 430 houses to the north of Elgin, which has already received planning permission.

"We are really keen to hear from the young people of Elgin " said Rebecca Russell of Pitgaveny Farms, "This is a chance to influence how a major new addition to Elgin could be developed. We have our views about creating a neighbourhood to be proud of, with lots of green spaces, links in to the beautiful Findrassie Woods, affordable well designed houses, cycle tracks, and footpaths and avenues of trees linking it all together. But we need to know what residents of all ages nearby think about street patterns, housing, of school provision, of new community facilities.

We want to hear ideas from as many people as possible so we can create a well-planned community that Elgin can be proud of. We want to make sure that Findrassie will provide Elgin with new, attractive, affordable homes and offer people space to work, learn and relax".

'Elgin and the NE of Scotland has a long history of planned neighbourhoods and towns' said Crinan Dunbar of Pitgaveny Farms. "Decisions made in what to build and where will have long term consequences for the communities in Elgin, so it is important that their views are made known early in the process.'



7.4 : COMMUNITY ENGAGEMENT

Pitgaveny held Open Evenings for staff, tenants and residents and regular briefings were provided to staff members.

Pitgaveny holds a Farm Open Day every other year and had a manned exhibition about Findrassie at the 2013 and 2015 events. Over 2,000 people attended in 2013 and over 3,000 people in 2015.

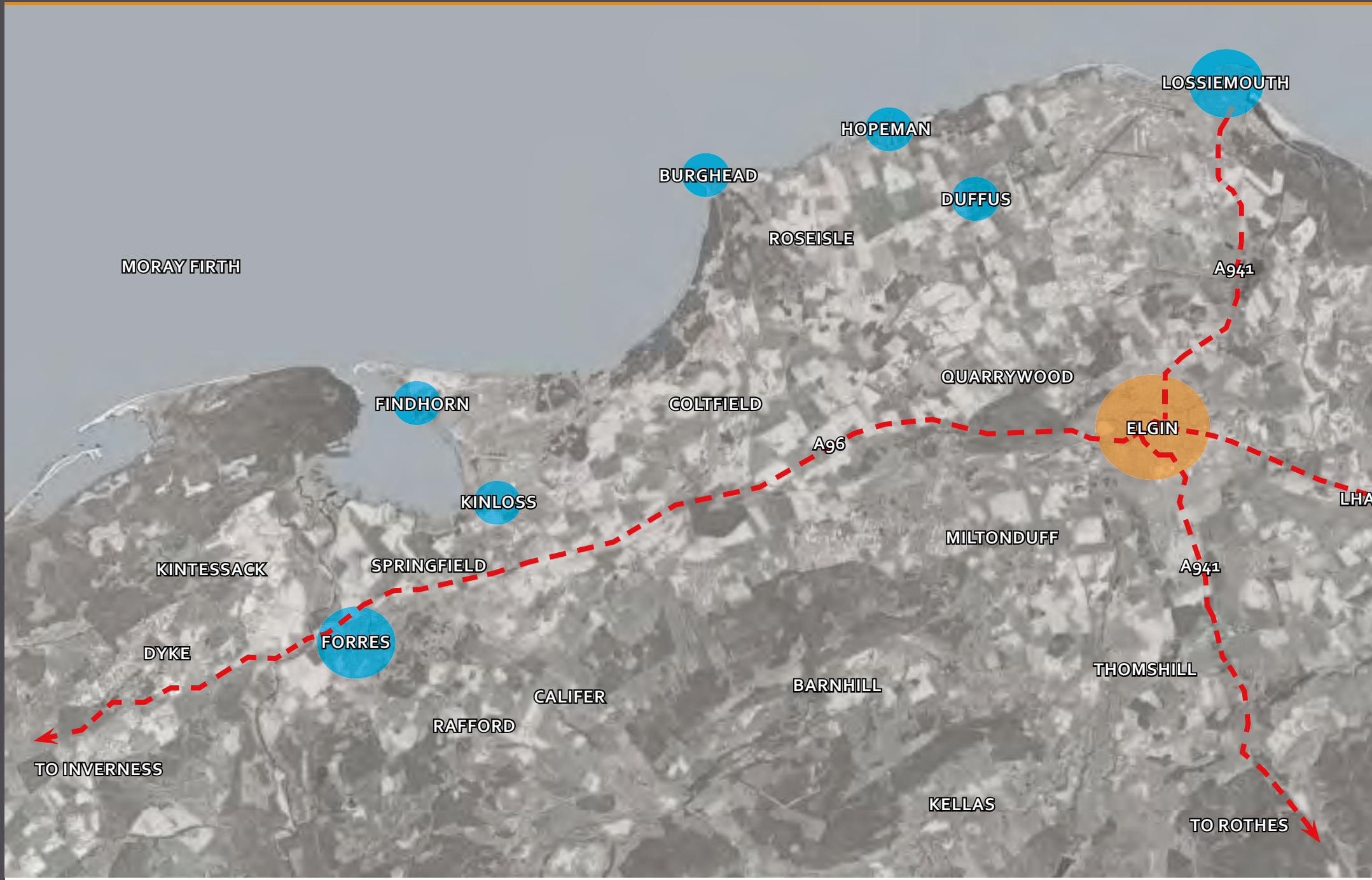
In tandem with the Farms Open Day, Pitgaveny also works with the Royal Northern Countryside Initiative to hold a schools open day for 13 year olds from Elgin Academy. 200 young people attend these open days and in 2015, a series of workshops on Findrassie were held with the school groups and were run by the Pitgaveny Design Team and The Moray Council.

The Elgin Youth Café worked with a Graphic Illustrator to imagine what this new neighbourhood might be like and how it can cater for the young people of Elgin.

Two waves of Public exhibitions were held for the wider community. In April 2015 two public events were held in St Giles Church on, Wednesday 22nd and Thursday 23rd of April 2015. A Consultation Workshop ran on Wednesday from 6pm to 8pm; and an Exhibition of the Masterplan proposals to date, ran from 1pm to 8pm on the Thursday. Adverts asked interested people to book for the Wednesday session, while the Thursday was a "drop-in" event where the Design Team appointed by Pitgaveny, looked for views and ideas from as many people as possible. In August 2015, The Moray Council ran the formal consultation period from 31 August to 23 October assisted by the Design Team and Landowners. This involved :

- For the various events, Posters were distributed around Elgin, and in public buildings and council offices advertising these events, and a leaflet was sent to every household in the Seafield and Bishopmill Primary School catchments.
- A website was set up to keep people updated about the masterplan www.findrassie.co.uk
- The Local papers covered the events and remarked on the hugely positive feedback received.
- A Detailed report on the Consultation Process is available on request.

8.o : CONTEXTUAL ANALYSIS



8.1: THE CONTEXT



The site lies on the northern edge of Elgin. One mile - or 20 minutes walking - from the town centre. It is 112 hectares in size, designated to be developed for housing (100 ha) and business uses (12ha). Whilst Findrassie is the largest allocation in the LDP other sites in Elgin will also deliver houses which is necessary to provide choice and meet demand. Furthermore policy H2 sets out the criteria where land within LONG term designations can be released. Similar to Findrassie, the Elgin South LONG designation will be reviewed through the next LDP. As such, Findrassie is one of a number of sites being developed to meet demand.

Largely contained between Lossiemouth road and Duffus road, it is largely accessible to public transport routes, the cycle network, and core footpaths.

Findrassie is of a scale, and in a location, which can significantly improve the northern edge and approaches to Elgin.

8.2: THE SITE

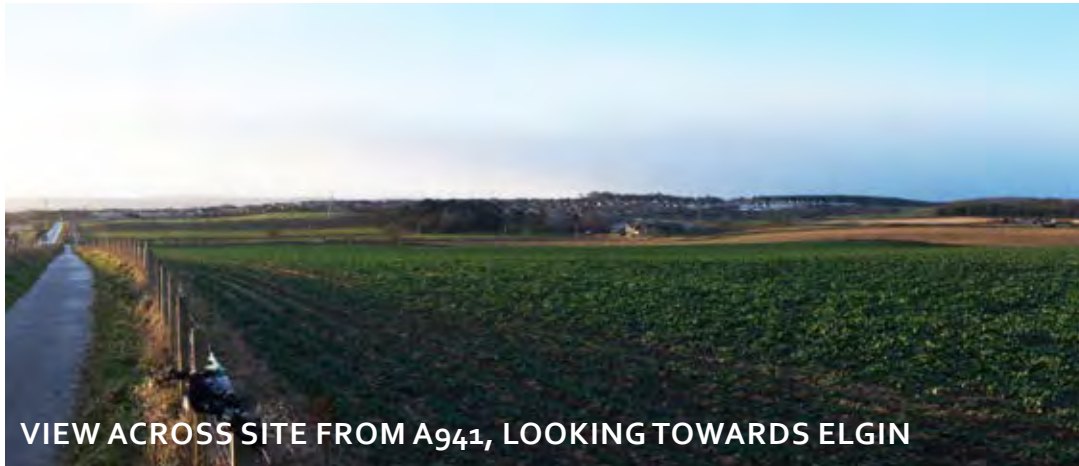




SEY BURN



COVESEA ROAD



VIEW ACROSS SITE FROM A941, LOOKING TOWARDS ELGIN



MYRESIDE STEADINGS



FINDRASSIE WOOD

8.2: THE SITE

The general topography of the area can be described as gently undulating with levels ranging from 25m along the northern and southern boundaries down to approximately 13m in the low lying central area of the site.

The site is contained in a 'bowl' shape by its contours, and defined by woodland boundaries. It is accessed by Lossiemouth Road, Duffus Road and Covesea road and adjoins the residential areas at Myreside Circle, and the new housing being built at Duffus Heights. It is "greenfield" farmed land, with only small areas of low lying ground along the route of the Sey Burn uncultivated. The Sey Burn flows through the site west to east and discharges into Spynie Loch, one mile away. There are only three dwellings in the whole area, one associated with the cattle steadings at Myreside Farm; Myreside House; and Myreside Cottage

8.3: SITE HISTORY

Since its medieval origins the City and “Royal Burgh” of Elgin has expanded organically in all directions. Historically the town originated to the south of the River Lossie on the higher ground above the floodplain, and the Findrassie Lands were traversed by a number of Historic routes between Forres, Elgin, Duffus and Spynie Palace. Until the sixteenth century, the shores of the Moray Firth were only one mile to the North, and in the eighteenth century, these lands were drained by the Spynie Canal system which greatly reduced the size of Loch Spynie. Today Elgin straddles the River Lossie, amalgamating Bishopmill to the north and New Elgin to the south, in an urban area of 23,000 people.

The housing estates of Bishopmill, which form much of north Elgin, were primarily formed during the sixties and seventies as a means of meeting Ministry of Defence and local authority needs. In the eighties further development occurred in the north with the large private estate at Woodlands being constructed. The nineties saw the town further extended to the north towards Myreside with a mix of private and MOD housing at Cove Sea Road.

From 1990 to 2010, the emphasis for new housing development moved to the south of the town (New Elgin) as part of an overall strategy to achieve a developer-funded distributor road system that links the A96 to the A941 (south) and improving internal traffic circulation to the town centre.



SPYNIE PALACE



BRACO'S BANKING HOUSE, 17TH CENTURY



EXISTING ELGIN RAILWAY STATION



SPYNIE CANAL



ST. GILES CHURCH + WAR MEMORIAL



DUKE OF GORDON
MONUMENT, 1839,



GLEN MORAY DISTILLERY

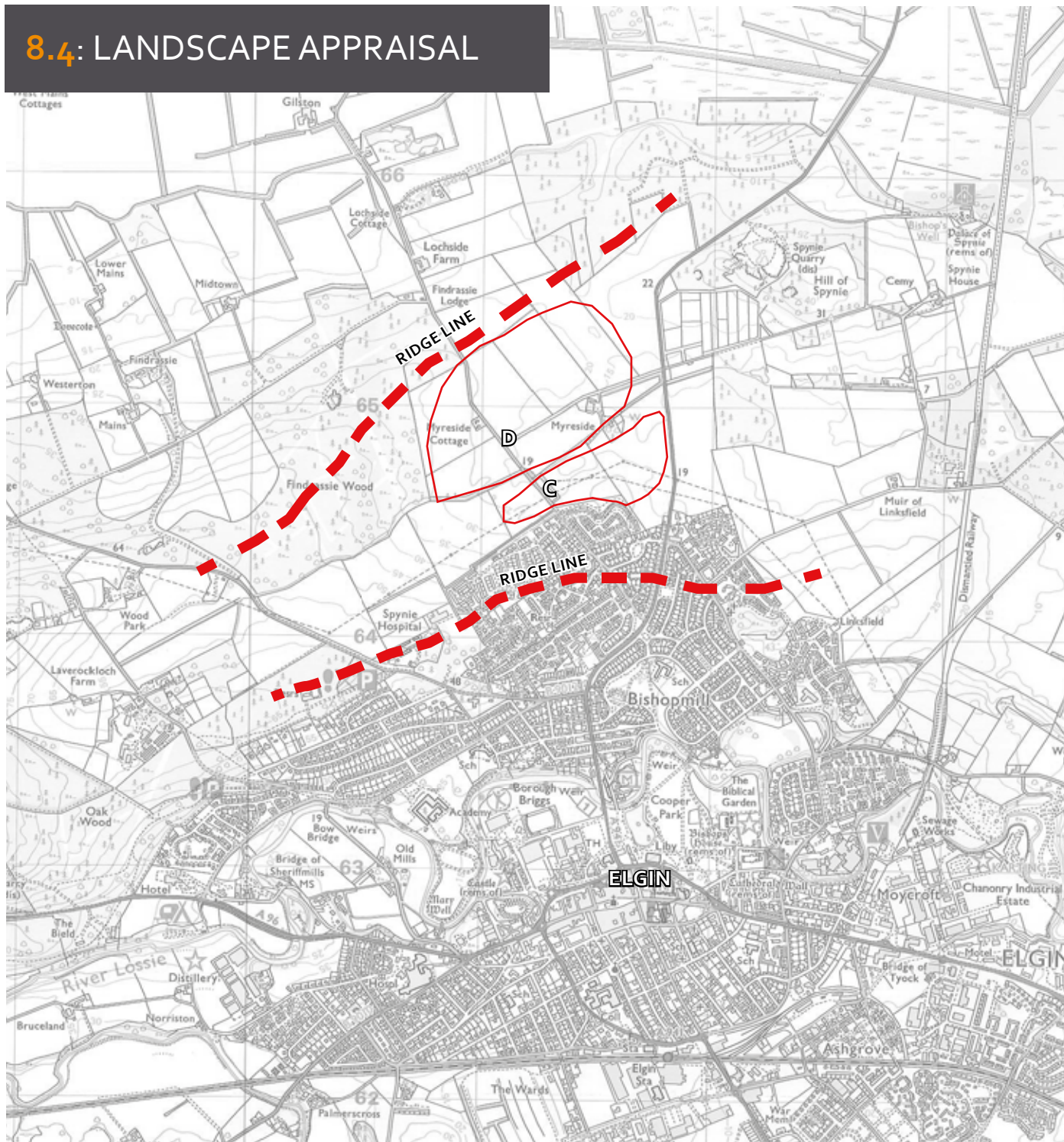


ELGIN CATHEDRAL, BURNT DOWN IN 1390



THUNDERTON HOUSE, 1500C

8.4: LANDSCAPE APPRAISAL





8.4.1: THE LANDSCAPE-LED APPROACH

1/ THE CHALLENGES OF THE LANDFORM

The site lies between two parallel ridge lines with a shallow valley between (See image A). The harsh urban edge of Elgin demonstrates the importance of woodland as an enclosing element on the horizon and also the need to introduce tree planting within the urban area to reduce its mass (See image B).

The base of the valley is dominated by the sub-station and associated overhead lines and pylons (See image C).

Views into the site reveal the more open and unenclosed fields of the south-facing slopes of the valley (See image D).

8.4.1: THE LANDSCAPE-LED APPROACH



8.4.1: THE LANDSCAPE-LED APPROACH

2/THE ZONES

Within the site there are three distinct zones of landscape character.

Zone 1

North facing slopes with the existing urban edge of Elgin as a strong visual element. Woodland forms an important background feature. The division between urban edge and farmland is abrupt without transition and as a result presents a poor impression of the town from the Lossiemouth road. A major sub-station and associated overhead lines and pylons are prominent features in the landscape.

ZONE 2

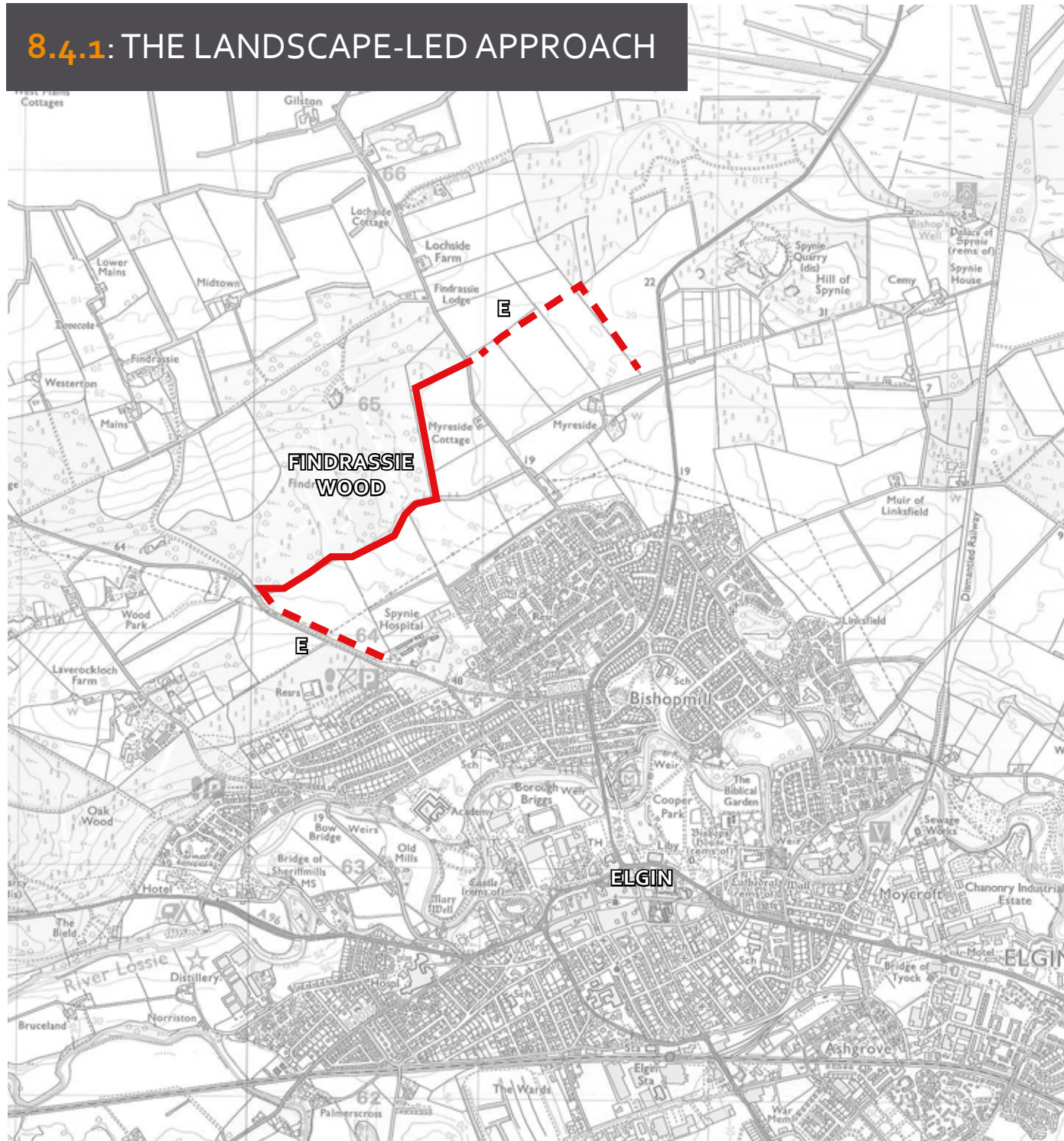
The open south facing fields sloping down to the valley floor of the Sey Burn, which is no more than a straight narrow ditch, the zone has few trees, except for new planting around the pond at Myreside Farm, and well established stands around Myreside House.

ZONE 3

The established woodland of the Findrassie ridge and across to the eastern boundary of Site I8. This acts as an enclosure to the site, but with a noticeable gap at the highest central point.



8.4.1: THE LANDSCAPE-LED APPROACH





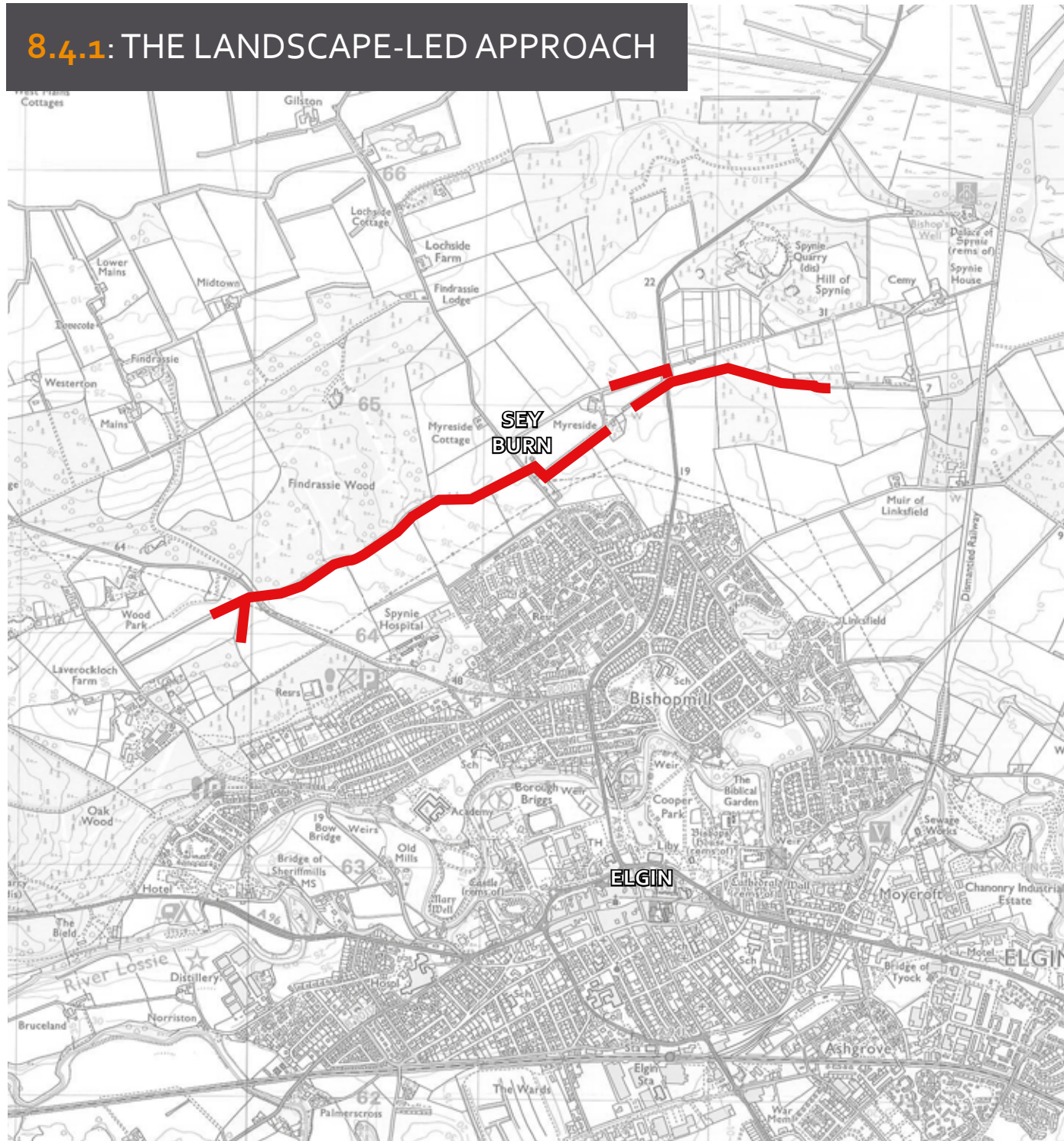
8.4.1: THE LANDSCAPE-LED APPROACH

3/VEGETATION

The importance of woodland to the character of the landscape, the setting of Elgin, and enclosure of the site is obvious (See images A, C and E). Findrassie Wood is also used for informal recreation (See image D). Nevertheless it was recognised that the main woodland blocks are commercial plantations which will be felled and replanted will significantly change the appearance of the landscape.

An approach was developed which recognised this future change by strengthening the edges of existing woodland blocks to transform them into permanent woodland. New planting should be carried out to complete enclosure of the site (See image E). The use of small woodland blocks (such as at Myreside) should be extended in the valley bottom (See image B) as well as individual trees and riparian planting at the stream side.

8.4.1: THE LANDSCAPE-LED APPROACH



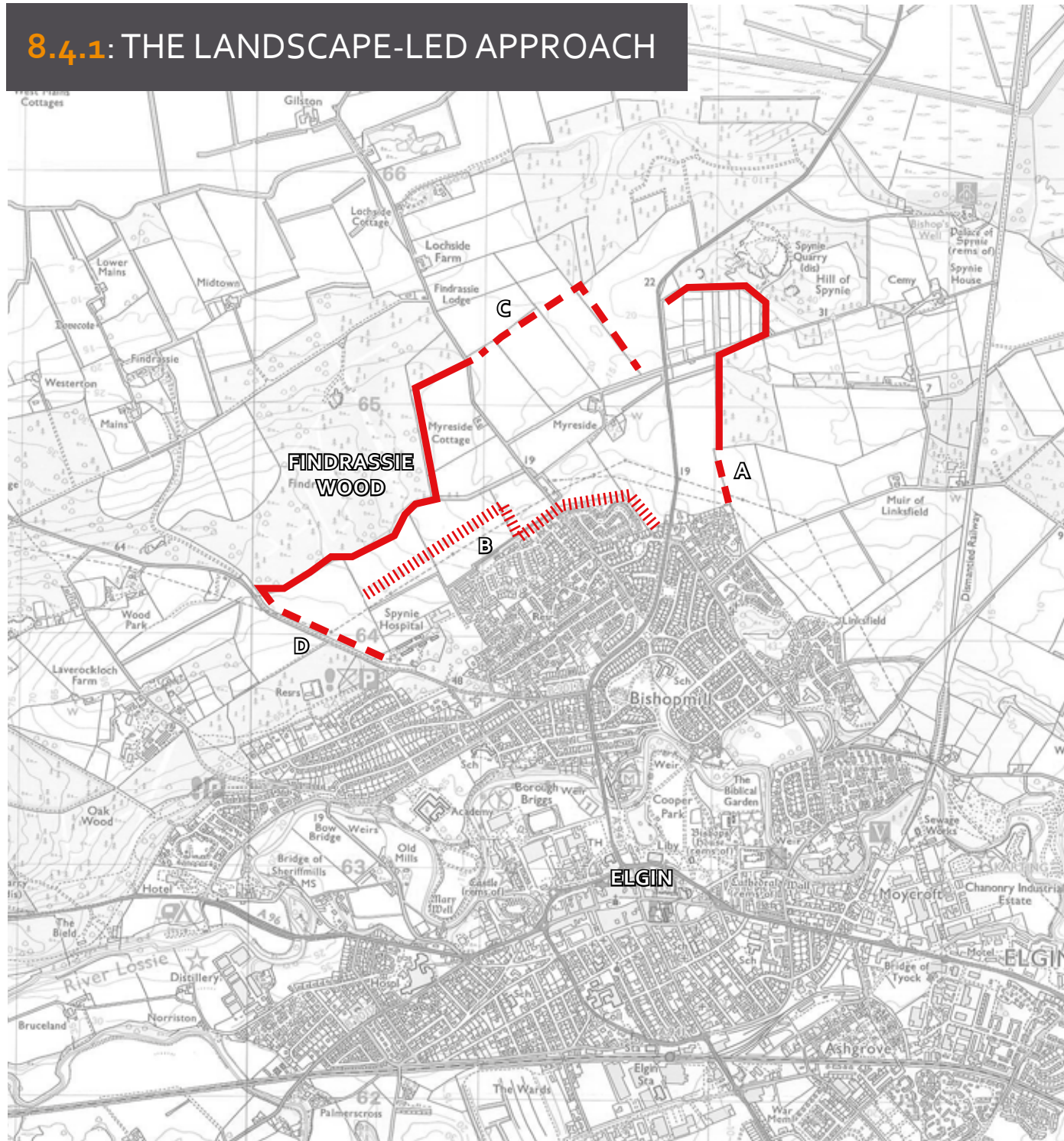
8.4.1: THE LANDSCAPE-LED APPROACH

4/WATER

The Sey Burn is an important but modest feature in the landscape (See image A). It will form an integral part of the SUDS system and has the potential to increase biodiversity as well as provide an attractive stream side walk which would link the eastern and western edges of the site (See image B).



8.4.1: THE LANDSCAPE-LED APPROACH





8.4.1: THE LANDSCAPE-LED APPROACH

5/ EDGES

The eastern edge of the site is well defined although further strengthening of the woodland planting at the base of Hill of Spynie would give better enclosure (See image A).

The southern edge of the site must blend into the existing housing and connect through to the surrounding countryside and Findrassie Wood with “green” corridors (See image B).

The northern edge of the site requires new woodland planting to strengthen enclosure on the horizon and treatment to the edges of existing blocks to turn them into permanent woodland (See image C).

The western edge of the site requires additional planting to strengthen the existing deciduous woodland in order to maintain the quality of the approach to the town on the B9012 (See image D).

8.4.1: THE LANDSCAPE-LED APPROACH





8.4.1: THE LANDSCAPE-LED APPROACH

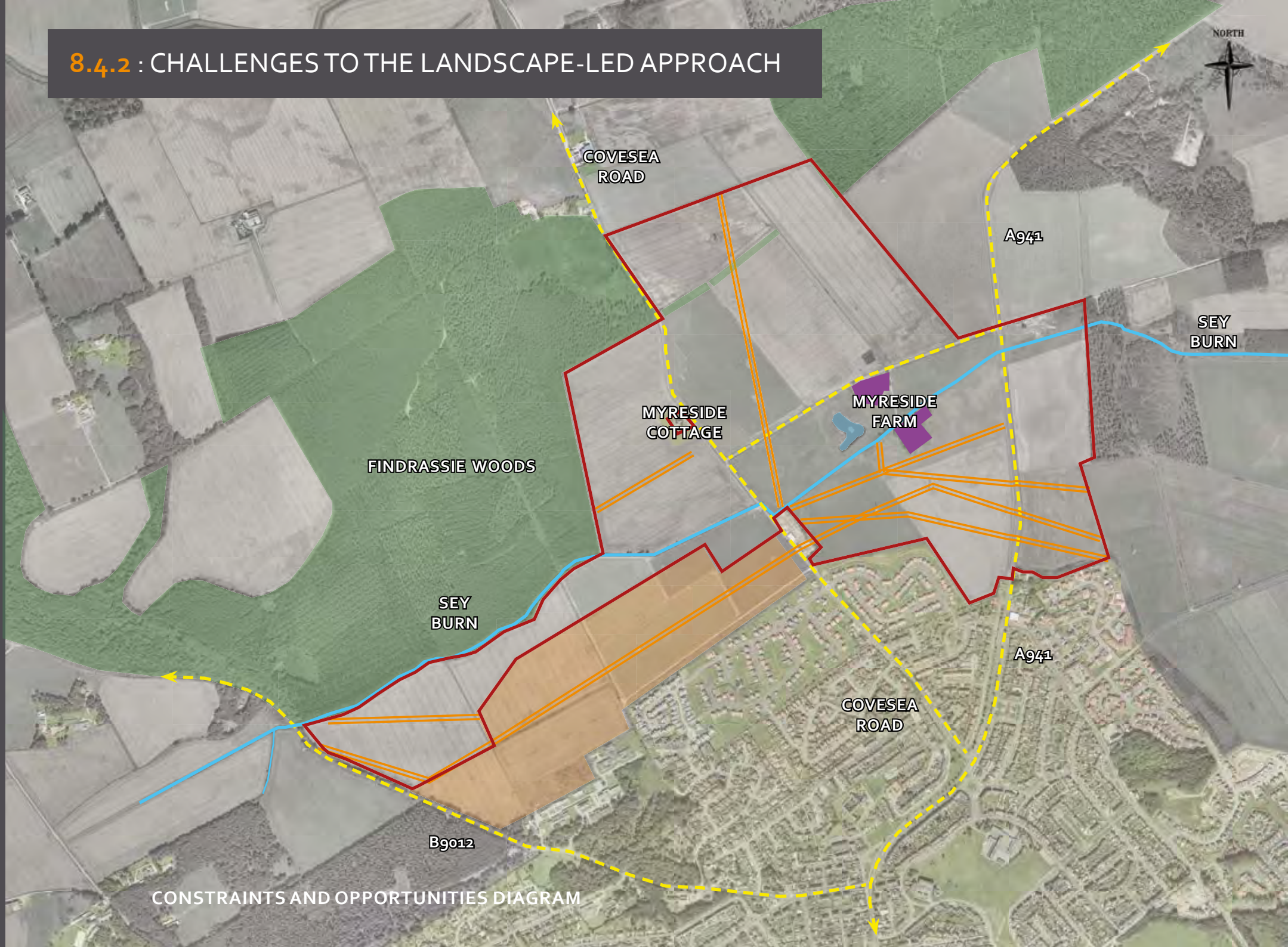
6/ LINKAGES

Links into the surrounding countryside should be one of the main attractions of Findrassie (See images C, D and E). A pedestrian and cycle route from east to west will form a major feature of the proposed development.

Links between the site and the town will be important can capitalize on the existing network which though not obvious, is in fact quite extensive (See images F, G, H and J).

The A941 approach to Elgin will form a major link which must be edged in woodland and merge into the boulevard tree planting which leads to the town centre (See images A and B).

8.4.2 : CHALLENGES TO THE LANDSCAPE-LED APPROACH



CONSTRAINTS AND OPPORTUNITIES DIAGRAM

8.4.2: CHALLENGES TO THE LANDSCAPE-LED APPROACH

The challenges to a landscape led approach to the site are primarily the overhead lines and pylons which cross the south east corner of the site, the primary electricity sub-station at the access point on Covesea Road, and the openness of the fields, particularly when seen from the main thoroughfare of Lossiemouth Road. There are secondary challenges, such as the harsh urban edge of the existing and still-to-be built housing areas on the Southern boundary, and the backdrops of plantation woodland, for which design plans will require to anticipate eventual felling and replacement.



EXISTING SUB STATION



EXISTING ROAD ACCESS NETWORK



OVERHEAD POWER LINES



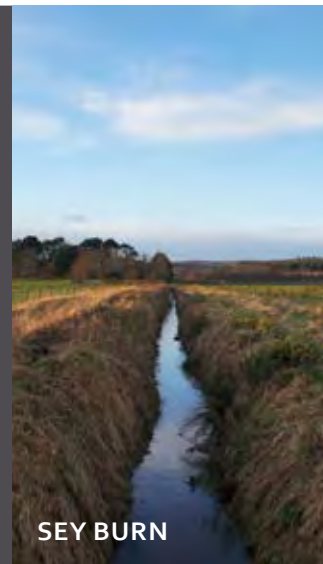
WATERCOURSE



POTENTIAL BUILDINGS TO BE RETAINED



ROBERTSON HOMES DEVELOPMENT SITE



SEY BURN

8.5 : SITE APPRAISAL

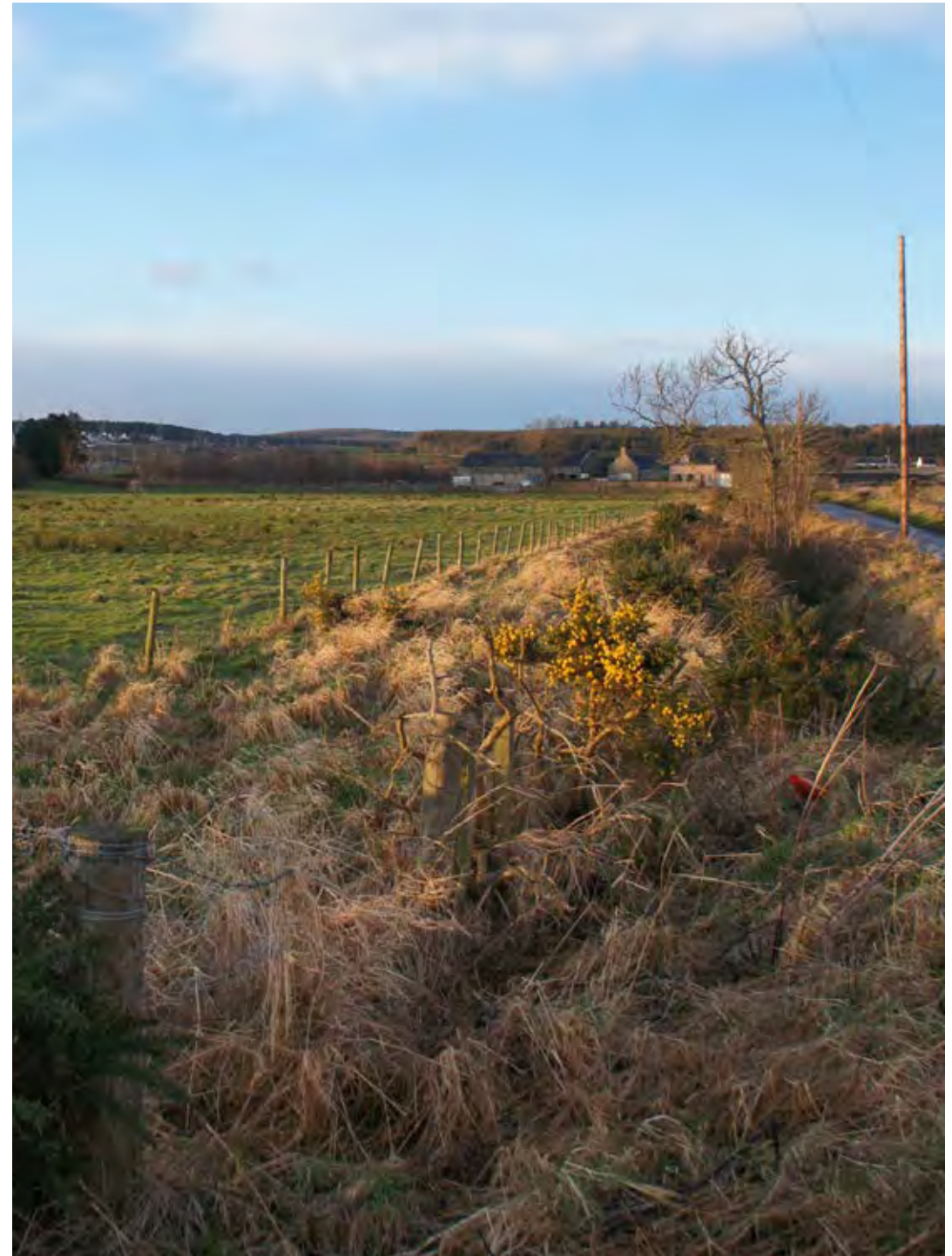
8.5.1 : GROUND CONDITIONS

Geotechnical desk study work has been completed and concluded:

- Bedrock in the area comprises sandstone of the Rosebrae Beds sequence.
- Superficial (soil) deposits comprise glacial tills (clays, sands and gravels) to areas west of Covesea Road and gravels, sands and silts to the east of Covesea Road and with an alluvial corridor following the line of the Sey Burn east of Covesea Road.
- Topsoil and subsoil conditions are reported locally to be dominated by sands and gravels and with silty sands around the burn corridor.
- Natural soils are observed to provide free draining conditions during passive rainfall events

Other notable ground condition related aspects include:

- No mining risk associated with the site
- Contamination risk is negligible as a result of the green field status and limited to impacts from agricultural practices
- No significant risks anticipated with respect to ground gas
- Standard development mitigation methods are anticipated to resolve any of the above aspects should detailed ground investigation works identify specific risks





8.5.2: EXISTING WATERCOURSES

The site area is crossed from east to west by the Sey Burn which forms the low point of the valley in which the site sits. The Sey Burn accepts field drainage from the surrounding fields as well as road drainage from Covesea Road and Myreside. Development surface water drainage from the existing Hamilton Gardens development discharges via a sustainable drainage pond located west of Covesea Road and development surface water drainage from existing residential development at Covesea Rise and Myreside Circus to the south discharges via a series of filter drains and trenches.

The Sey Burn will be incorporated as a positive feature in the site layout for the developing masterplan.

It will be necessary to cross the Sey Burn in order to link development parcels with good connectivity. In accordance with SEPA policy "Construction of River Crossings Good Practice Guide" where crossing of the watercourse cannot be avoided they will be undertaken utilising a solution that has little impact on the bed or banks of the watercourse. Crossings will be subject to an authorisation application to SEPA under the "Water Environment (Controlled Activities) (Scotland) Regulations 2011.

Engineering activities such as culverts, bridges, watercourse diversions, bank modifications or dams should be avoided unless there is no practicable alternative. However, if engineering activities are proposed in the water environment, a map of the location of such activities should be included with all planning submissions. A systematic table detailing the justification for the activity and how any adverse impact will be mitigated should also be included. If existing features are modified, SEPA will welcome opportunities for betterment for example, de-culverting or re-meandering artificially straightened features.

Flood Risk Assessments must be completed to demonstrate the flood envelope for the 200 year (0.5% annual average probability) plus a 20% allowance for climate change. Each Flood Risk Assessment shall include the agreed SEPA and Local Authority Flood Officer sensitivity analysis and methodology. Consultation with SEPA and Local Authority Flood Officer will be required in order to agree the scope prior to Flood Risk Assessment being carried out.

8.5.3: TOPOGRAPHY

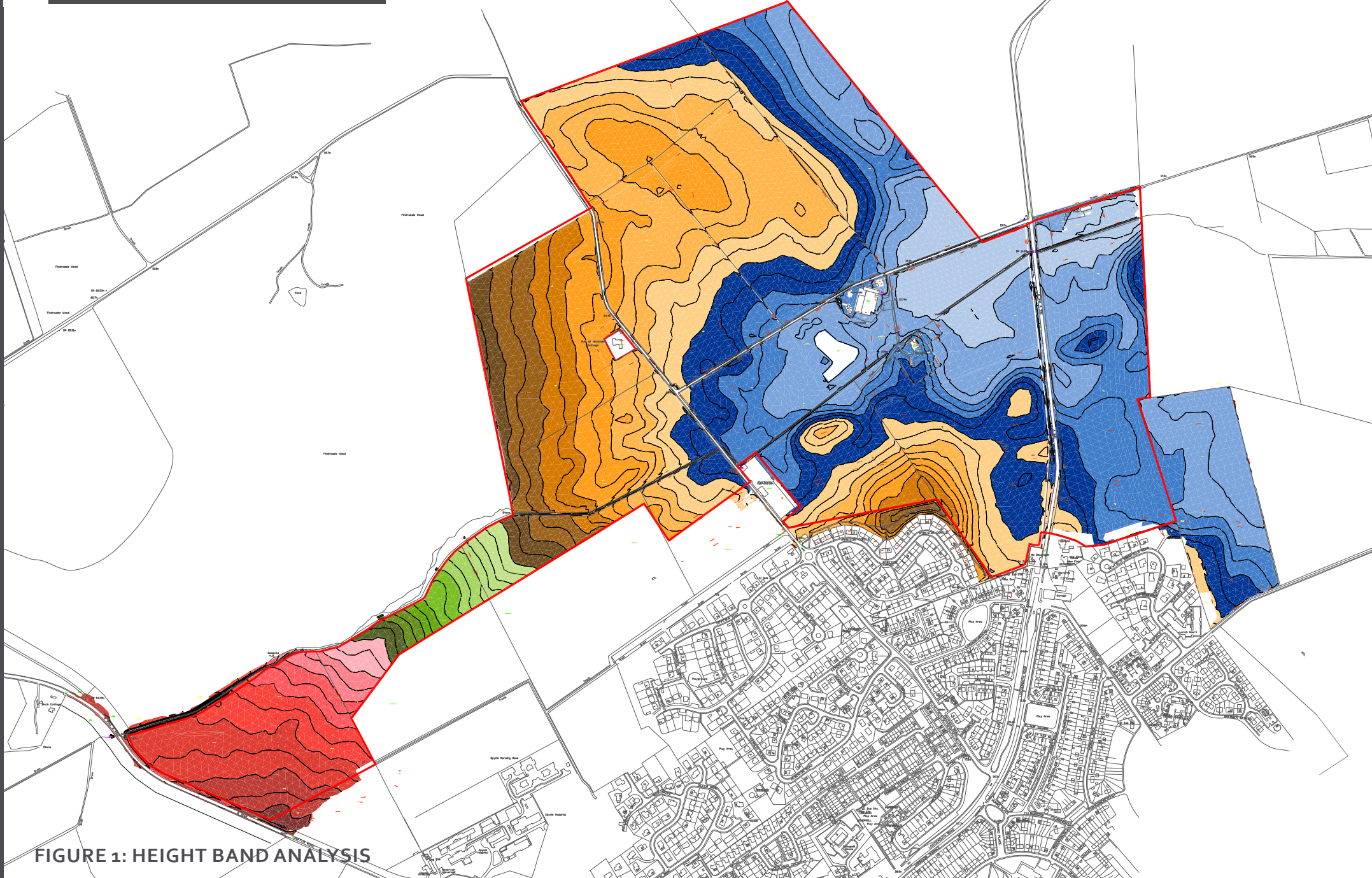


FIGURE 1: HEIGHT BAND ANALYSIS



FIGURE 2: SLOPE ANALYSIS

FIGURE 1 KEY:

HEIGHT RANGES (mAOD)

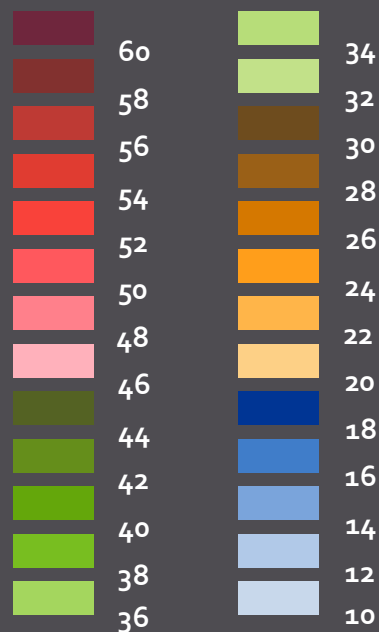
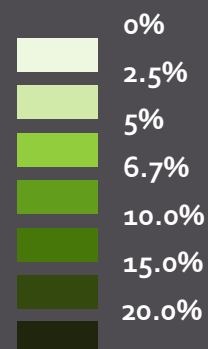


FIGURE 2 KEY:



8.5.3: TOPOGRAPHY

The topography of the site area does not present any significant challenges to achieving development platforms and profiles for residential development of the scale proposed at Findrassie.

The topography of the site area has been established via detailed survey and analysis of slopes, contours, heights and features. The topography describes a gently undulating shallow valley with predominantly shallow gradients with localized areas of steeper gradient in areas to the south and east and around the watercourse line.

The topographical survey was specified to include measurements for electricity pylon tower heights as well as Sey Burn cross sectional areas and dimensions in order to inform aspects of the technical reporting, electrical transmission tower line assessments as well as providing three dimensional survey data for the site area for the purposes of ground modelling and cut & fill analysis.

The detailed survey builds on previous topographical analysis work using ordnance survey sourced digital contours and has confirmed the following with respect to the site:

- Contour is that of a gently undulating landform with a low point valley feature described by the Sey Burn corridor
- Site gradients are predominantly in the range 2.5% to 5% (1 in 40 to 1 in 20) and as such represent natural gradients within which development can proceed without the need for extensive earthworks
- Localised feature areas in the form of low rise mounds with gradients up to 10 % (1 in 10) exist around the Myreside area and areas north of Myreside Circus and will be able to be reshaped or incorporated within the designed landform and without the need for removal of soil materials from site.

Topography analysis plans of the existing landform are presented in Figure 1 and 2 (opposite).

8.6: ELGIN'S TOWNSCAPE



NETWORK OF OPEN SPACES - LOSSIE GREEN



HISTORIC BUILT FORM - BRACO'S BANKING HOUSE



URBAN LANDMARKS - ST GILES CHURCH



ACTIVE FRONTAGES + POSITIVE STREETSCAPES - HARRISON TERRACE

8.6: ELGIN'S TOWNSCAPE

The town grew with the establishment of the Morayshire Railway in 1852 however it was not until the 20th Century that the villages of Bishopmill (north) and New Elgin (south) were incorporated into the town. The settlement at Bishopmill grew quickly with people moving away from the town centre. In 1936 a new primary school was required to accommodate the increase in population and is still in operation today. The grid pattern of streets and lanes within Bishopmill evolved either side of north street along East / West High Street, East / West Back Street and Blantyre Place. This pattern was repeated in later developments and should inform the new masterplan at Findrassie. The architecture of the area is very much traditional with simple forms and materials. The housing is a mix of 1 and 2 storey cottages and semi-detached and terraced housing. The materials are stone, render, slate and tiles. The boundary treatments to houses within the area are important in defining private gardens and public realm.



FOCAL POINTS

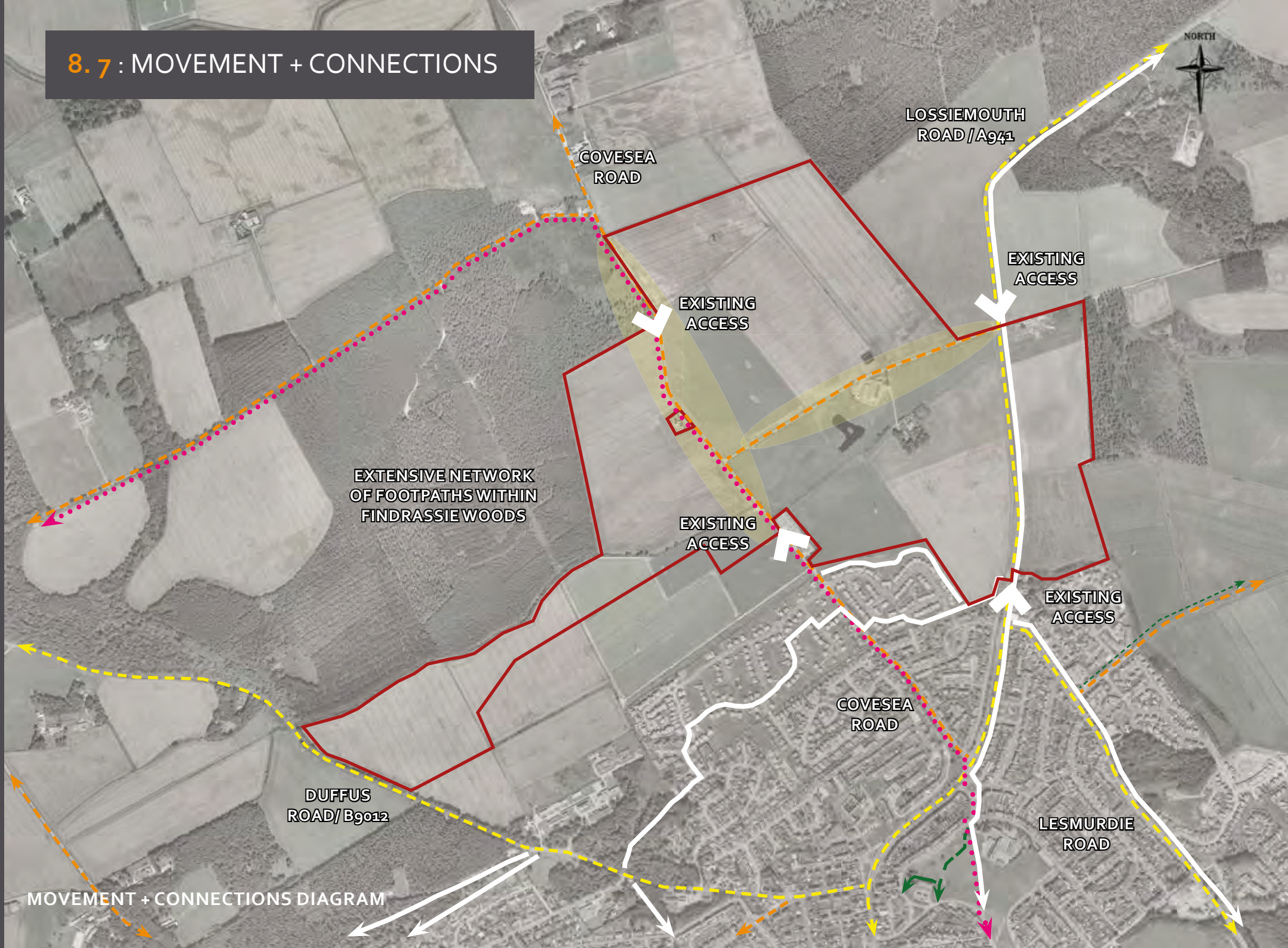


SUCCESSFUL URBAN REALM



AERIAL VIEW OF ELGIN'S TOWN CENTRE

8.7 : MOVEMENT + CONNECTIONS



MOVEMENT + CONNECTIONS DIAGRAM



8.7: MOVEMENT + CONNECTIONS







The site is well served by the existing surrounding road network. Existing access points are from the A941 to the east and Covesea Road from the north and south. Access from the A941 is along an unclassified road, which is to be maintained, but may require upgrading.

To the east, the A941 links Lossiemouth with Elgin town centre and the A96. The B9012, on the western boundary links Hopeman and Duffus to the east. Covesea road runs north to south through the site and provides single track routes to both these roads. All of these routes provide the site with access to neighbouring shops and local amenities.

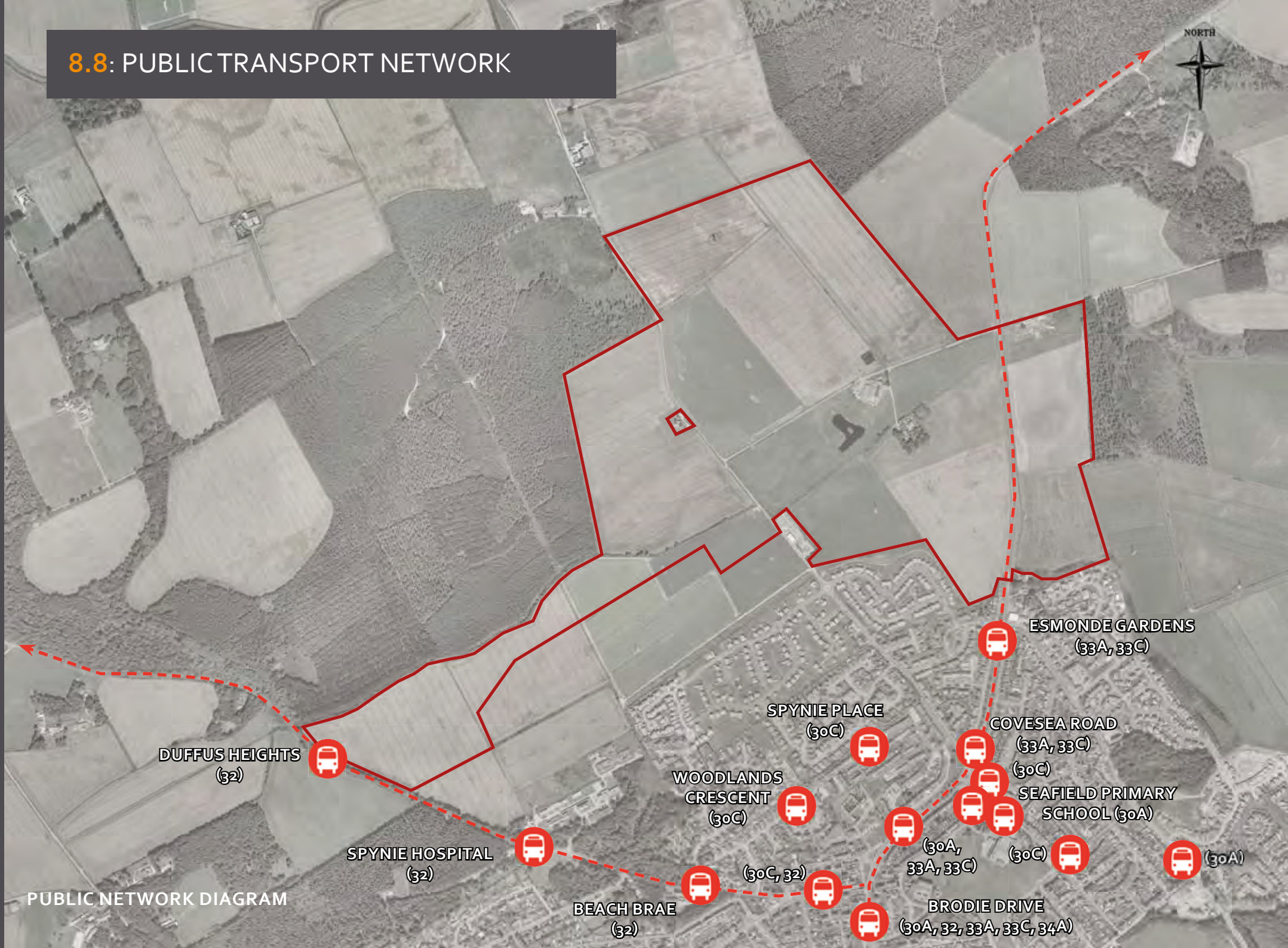
The roads within the site are generally well maintained and in good order. Existing stone dykes should be retained and where road up-grading is required, existing materials should be retained and used in the rebuilding of the stone dykes to ensure that this feature is not lost.

The site offers the potential to greatly improve existing footpaths and cycle networks. A new footpath network will be implemented along the Sey Burn and through proposed open spaces to integrate the new development into the surrounding area. This will run from the existing cycleway in the east to the tracks within Quarrel Wood in the west. The site is well located for cycle access to Elgin town centre, railway station and farther afield to Lossiemouth.

The framework also presents an opportunity to inform emerging housing to the south and improve the footpath links through these neighbouring developments.

-  PRIMARY LOCAL ACCESS ROAD NETWORK
-  SECONDARY LOCAL ACCESS ROAD NETWORK
-  CORE PATHS
-  WIDER PATH NETWORK
-  NATIONAL CYCLE ROUTE
-  RETENTION OF EXISTING STONE DYKE WALLS

8.8: PUBLIC TRANSPORT NETWORK





ESMONDE GARDENS BUS STOP



EXISTING BUS STOPS



PRIMARY BUS ROUTE

30 A+C

BISHOPMILL CIRCULAR

32

ELGIN - BURGHEAD VIA DUFFUS AND HOPEMAN (+ RETURN)

33 A+C

PINEFIELD - MANITOBA, BILBOHALL - ELGIN BUS STATION - LOSSIEMOUTH (+ RETURN)

34 A

ELGIN BUS STATION - KINGSTON (+ RETURN)

8.8: PUBLIC TRANSPORT NETWORK

The existing public transport network provides good access to the north of Elgin and further afield to Lossiemouth, Duffus, Burghead and Hopeman. The development at Findrassie must provide a framework within which the local public transport network will be improved. Providing access not only to the new neighbourhood but reinforcing existing services through the surrounding residential area. The aim should be to ensure that all residents live within 5mins of a bus route.

It will be necessary for public transport proposals to be developed in tandem with the phasing of the development. Early phases that may lie close to existing radial routes can benefit from existing public transport services, but an east west route capable of supporting services must be provided and service diversions will be required at the appropriate point in the development phasing.

8.9: SURROUNDING BUILT HERITAGE

NORTH



FINDRASSIE DOVECOT

FINDRASSIE HOUSE +
GATEPIERS

WOODLANDS
FARM CONVERTED
HORSE GANG

HARRISON TERRACE

LESMURDIE HOUSE
DOVECOT

42 NORTH STREET
ELM BANK

LESMURDIE HOUSE



1.



2.



3.



4.



5.

8.9: SURROUNDING BUILT HERITAGE

There are no specifically protected sites within the masterplan area, but there are a number of important Heritage sites and listed buildings in the site's surrounding area, mainly within the urban boundary of Elgin. The closest to the site is the 19th century converted horse gang at Woodlands Farm.

Lesmurdie House and Dovecot, and Findrassie House and Dovecot, are all about a kilometre away.

Further a field there are numerous sites and buildings which define the character and history of this area, particularly so in the historic centre of Elgin. Within sight of Findrassie are the important historic landmarks of Spynie Palace and Duffus Castle, and the Spynie Canal System. Of most significance to the urban design 'benchmarks' for Findrassie is Harrison Terrace in Bishopmill.

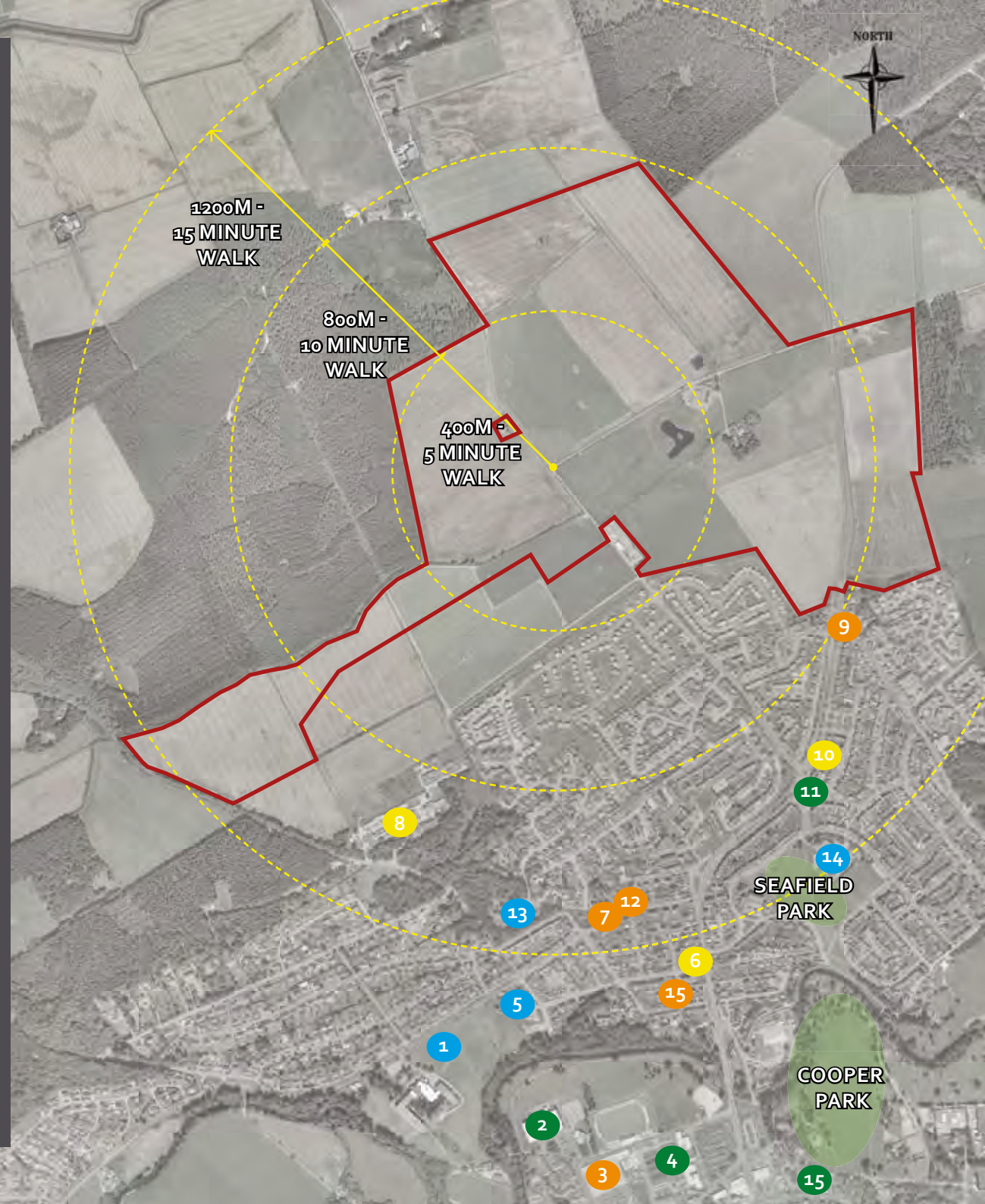
1. FINDRASSIE DOVECOT
2. WOODLANDS FARM CONVERTED HORSE GANG
3. HARRISON TERRACE
4. LESMURDIE HOUSE
5. LESMURDIE HOUSE DOVECOT

8.10: ACCESS TO LOCAL AMENITIES

AMENITIES KEY:

1. ELGIN ACADEMY
2. MORAY LEISURE CENTRE
3. TESCO
4. ELGIN SPORTS + COMMUNITY CENTRE
5. BISHOPMILL PRIMARY SCHOOL
6. BISHOPMILL PHARMACY
7. COSTCUTTER
8. SPYNIE DENTAL CENTRE
9. SERVICE STATION + SHOP
10. BISHOPMILL DENTAL CENTRE
11. BONNIE EARL PUB
12. POST OFFICE
13. BEACH BRAE EDUCATION CENTRE
14. SEAFIELD PRIMARY SCHOOL
15. ELGIN LIBRARY
16. BISHOPMILL HALL

- EDUCATION
- RECREATION + LEISURE
- HEALTH
- RETAIL/ OTHER
- PARK





1. ELGIN ACADEMY



2. MORAY LEISURE CENTRE



3. TESCO



9. SERVICE STATION + SHOP



15. ELGIN LIBRARY

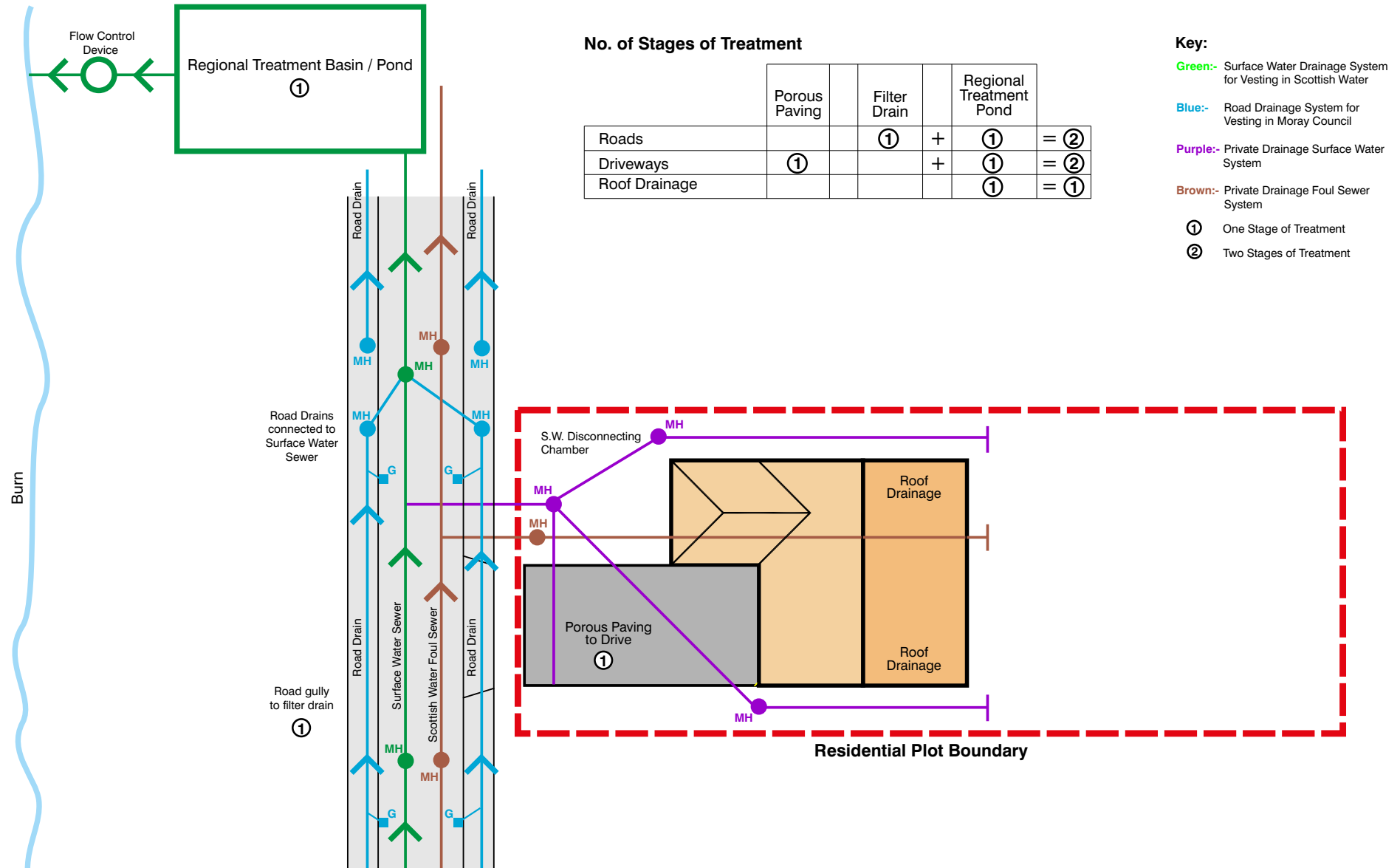
8.10: ACCESS TO LOCAL AMENITIES

The site is well served by existing amenities within the surrounding area. The majority of houses within the new neighbourhood are within walking distance of the existing primary schools at Bishopmill and Seafeld and the newly opened Elgin Academy on Morriston Road.

Slightly further afield is Cooper Park and the existing town centre which are approximately a 20 / 25 minute walk respectively from the site. The area is also close to nearby shops and retail outlets such as Scotmid, Costcutter and the Buccaneer Service Station on Lossiemouth Road. The superstore at Lossie Green is also located nearby.

The site is also served by a number of other essential services such as community facilities and health centres.

9.0 : ENGINEERING



RESIDENTIAL DRAINAGE - SURFACE WATER MANAGEMENT TRAIN

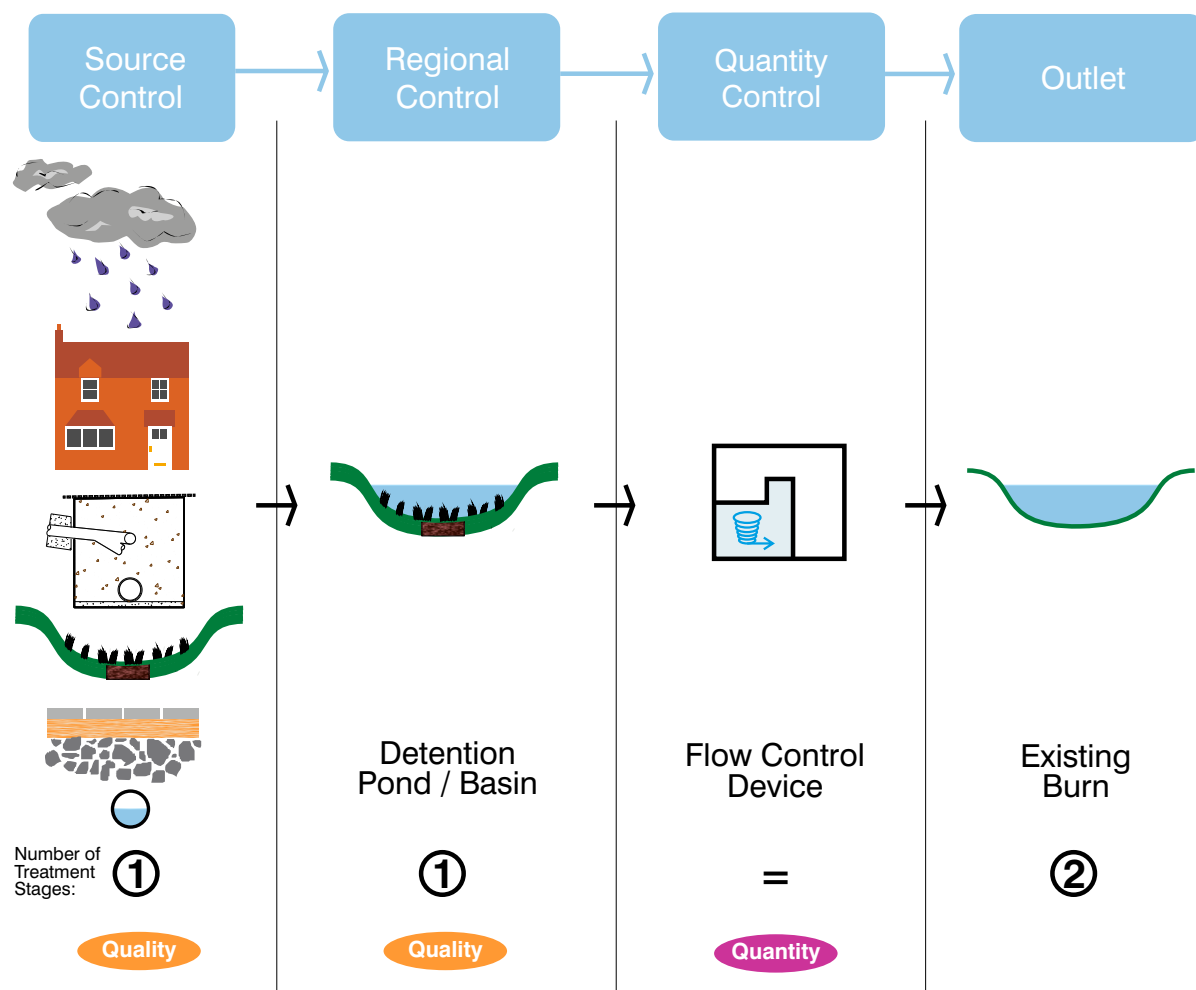
9.1: R DRAINAGE

9.1.1: SURFACE WATER

The surface water drainage systems shall be designed in accordance with the principles of Sustainable Drainage Systems (SUDS). The SUDs systems will replicate the natural drainage of the existing surface water catchments and mitigate the effects of surface water run-off from the development by:

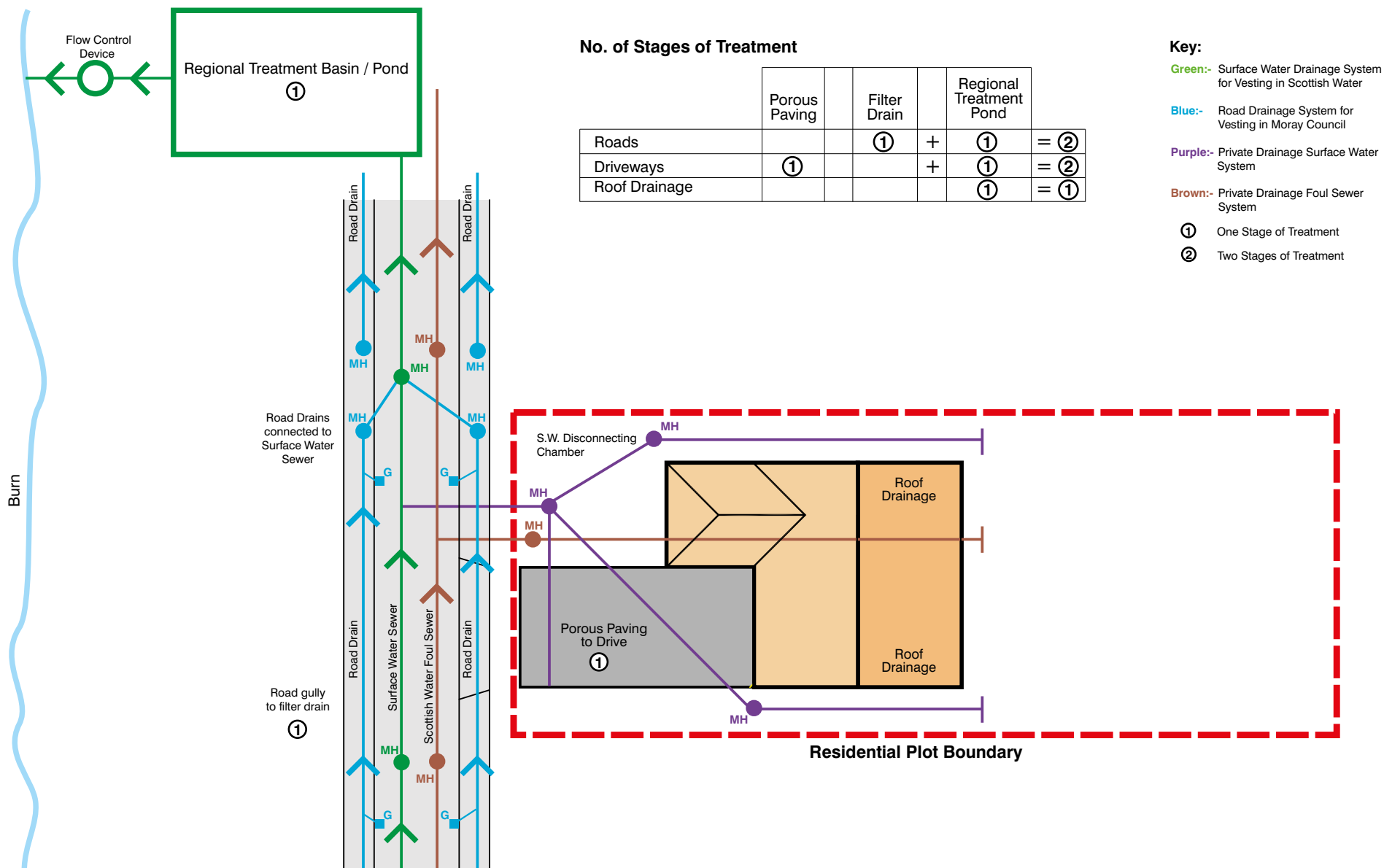
- Managing and restricting run-off rates to mitigate the risk of downstream flooding;
- Encouraging natural groundwater recharge where appropriate;
- Reducing pollutant concentrations in the run-off;
- Contributing to the enhanced amenity and aesthetic value of developed areas;
- Providing habitats for wildlife in urban areas and opportunities for biodiversity enhancement;
- Improving water quality entering the Spynie Loch catchment by removing agricultural land catchment.

These criteria shall be achieved by following a Management Train approach and incorporating SUDS techniques in series, from source to discharge, in order to mitigate pollution potential and to control run-off rates. Where appropriate, SUDS are to be incorporated within public open space and provide amenity benefits. The Management Train and levels of treatment required for the different development types at Findrassie have been illustrated as follows.

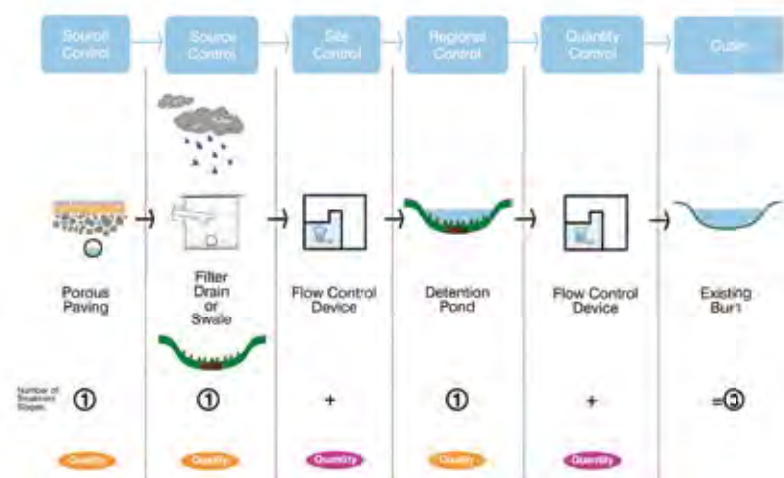


RESIDENTIAL DRAINAGE - SURFACE WATER MANAGEMENT TRAIN

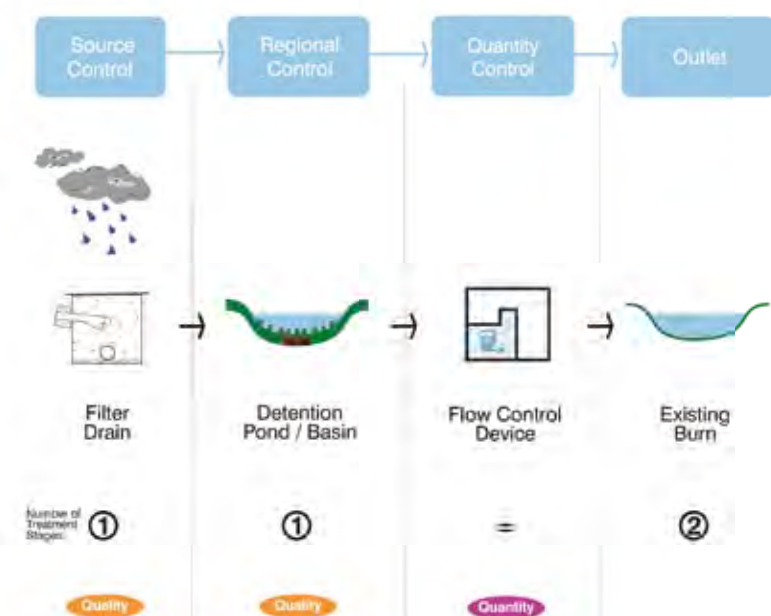
5.5: DRAINAGE



INDUSTRIAL DRAINAGE - SURFACE WATER MANAGEMENT TRAIN



INDUSTRIAL DRAINAGE - SURFACE WATER MANAGEMENT TRAIN



ROADS DRAINAGE - SURFACE WATER MANAGEMENT TRAIN

The drainage system shall be analysed to assess performance during extreme rainfall events and overland flow routes are to be considered to ensure that run-off is managed appropriately. Finished site levels will be designed to prevent water entering buildings or restricting access for emergency vehicles.

The surface water drainage systems will be designed in accordance with the following documentation:

- SUDS Manual (CIRIA C697)
- PAN 61 Planning and Sustainable Urban Drainage Systems
- PAN 79 Water and Drainage
- Sewers for Scotland, 3rd Edition, June 2015
- Moray Council Development Guidelines,
- National Roads Development Guide published by SCOTS and
- SUDs for Roads published by SCOTS.
- Sustainable Urban Drainage Systems WAT RM-o8 Regulatory Method published by SEPA

The SUDS solutions will also require to satisfy the adoption and maintenance requirements of Scottish Water and Moray Council as well as other organizations and agencies who may have an interest, ownership or control of the facility. The proposed approach can be summarised as follows:

- Where subsoil infiltration permits and where practicable, source control measures such as soakaways and/or porous paving shall be incorporated.
- Surface water sewers, road drains, filter drains and swales will, where possible, be located within the proposed roads, private drives, areas of open space and within road verges.
- Individual houses and properties will each be connected to the surface water sewers via a disconnecting chamber.
- The site being drained in catchments based on the existing topography, with flows from each area be directed to an appropriate site control such as a detention basin or pond. The basins and ponds will discharge to the existing watercourses at a controlled rate no greater than the current greenfield run-off rate (3.55 litres per second).
- Grass conveyance swales and/or filtration trench swales shall be considered, where gradients permit, upstream of the basins and ponds to provide additional levels of treatment and sediment removal.



SUCCESSFUL SUDS INTEGRATION WITHIN LANDSCAPE AND WITH POSITIVE FRONTAGE / NATURAL SURVEILLANCE



SUCCESSFUL SUDS INTEGRATION WITHIN LANDSCAPE



EXAMPLE OF SUCCESSFUL DRY SUDS BASIN

Roofs are to be provided with a minimum of one level of surface water treatment, whilst car parks, service yards and roads are to be provided with a minimum of two levels of treatment.

The surface water drainage system will drain and treat and run-off as follows:

- Run-off from roof areas will drain to a basin or pond via filter drain, grass swale and/or surface water sewer.
- Run-off from individual plot driveways will, where practicable, be drained to ground at source, via a soakaway / filter trench and / or porous surfacing. Where this cannot be achieved, driveways will drain to a basin or pond via the surface water sewer and will have source control provided by grass swale, filter drain, porous paving or other SUDS measure.
- Run-off from parking areas and commercial service areas will, where practicable, be drained to porous surfaced areas and discharge to a basin or pond via a grass swale and/or surface water sewer.
- Run-off from roads will drain to a basin or pond via filter drains and / or grass swales and/or road drainage and/or surface water sewers.
- For all developments, run-off from areas subject to particularly high pollution risk (e.g. yard areas, service bays, fuelling areas, pressure washing areas, oil or chemical storage, handling and delivery areas) should be minimised and directed to the foul sewer and subject to technical approval by Scottish Water.

As part of the design of the masterplan adequate space based on a percentage of hard standing of development has been assigned to SUDs features.

SUDS regional treatment type facilities illustrated.

9.1.2 : CONSTRUCTION PHASE DRAINAGE

In order to prevent construction run-off from polluting receiving waters (e.g. due to suspended silt or discharges such as cement slurry or oil) a surface water management strategy will be prepared for each construction phase as part of the Construction and Environmental Management Plan (CEMP). This strategy will incorporate mitigation such as:

- Localised interception of surface water run-off by temporary ditches or channels around the area of works;
- Ponds to provide settlement and filtration; and
- Protection of the permanent drainage system from silt and pollution.

In addition to the above, plant fuelling and storage areas will be carefully sited and bunded as necessary to avoid any potential pollution.

The following guidance documents will be considered in preparing the construction phase surface water management strategy;

- SEPA PPG 6 : Working at construction and demolition sites
- SEPA PPG 5: Works and maintenance in or near water





9.1.3: WASTE WATER DRAINAGE

Waste water drainage provision has been considered as part of the masterplan for the development. The following guidance was reviewed when considering the waste water strategy for the development:

- PAN 79: Water and Drainage
- SEPA Policy and Supporting Guidance on Provision of Waste Water Drainage in Settlements

The waste water solution for the development will discharge to the existing Scottish Water sewerage system.

In order to establish the ability of the existing Scottish Water sewer network to accept foul drainage from a development area, a Pre Development Enquiry to Scottish Water is required to establish whether or not a Drainage Impact Assessment (DIA) is required. In the case of Findrassie it is almost certain that such an assessment will be required.

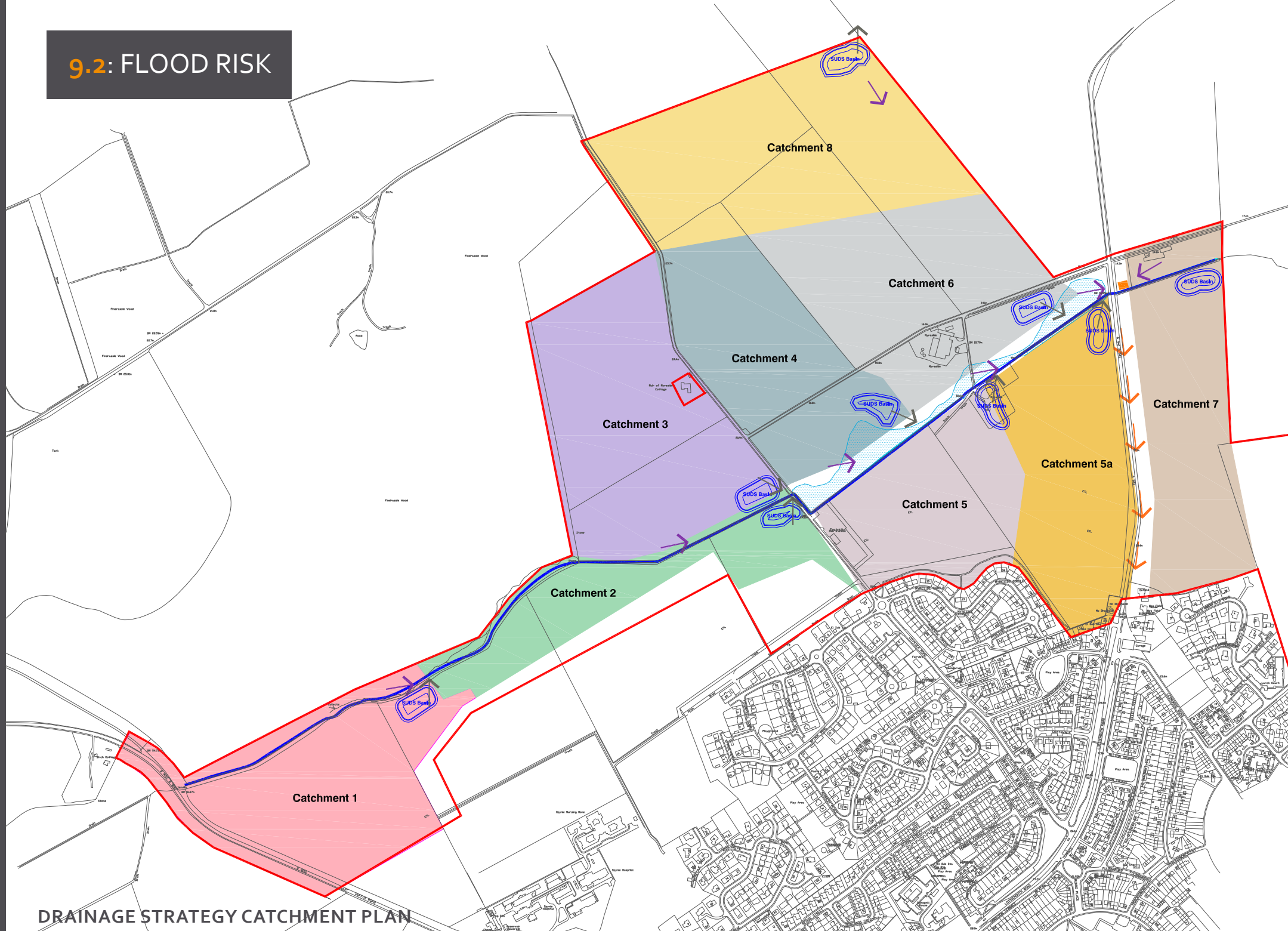
The DIA will identify any upgrades to the existing network that may be required to serve the development.

A development of the scale of Findrassie would usually suggest that a staged/phased approach to DIA reporting is employed.

Discussion between Pitgaveny and Scottish Water has confirmed that the Waste Water Treatment Works at Lossiemouth has surplus capacity.
















Development of the site is likely to require foul sewer pumping station(s) on the basis that topography suggests that gravity connections to the existing Scottish Water network are unlikely to be feasible.

9.2: FLOOD RISK



DRAINAGE STRATEGY CATCHMENT PLAN



	FINDRASSIE SITE BOUNDARY
	CATCHMENT 1 = 14.6HA ATTENUATION REQUIRED = 4210M CUBED
	CATCHMENT 2 = 8.6HA ATTENUATION REQUIRED = 2405M CUBED
	CATCHMENT 3 = 16.5HA ATTENUATION REQUIRED = 4979M CUBED
	CATCHMENT 4 = 11.8HA ATTENUATION REQUIRED = 3740 M CUBED
	CATCHMENT 5 = 9.54HA ATTENUATION REQUIRED = 2900M CUBED
	CATCHMENT 5A = 11.9HA ATTENUATION REQUIRED = 3544M CUBED
	CATCHMENT 6 = 14.2HA ATTENUATION REQUIRED = 4210M CUBED
	CATCHMENT 7 = 11.6HA ATTENUATION REQUIRED = 3544 M CUBED
	CATCHMENT 8 = 17.64HA ATTENUATION REQUIRED = 4916 M CUBED
	FLOOD ENVELOPE
	SURFACE WATER OUTFALL ROUTE
	FOUL WATER OUTFALL ROUTE
	RISING MAIN ROUTE
	PUMPING STATION LOCATION

9.2: FLOOD RISK

A detailed Flood Risk Assessment has been completed and identifies flood envelope mapping to allow detailed masterplanning to be developed with development areas shown clear of flood risks.

Flood Risk Assessments must be completed to demonstrate the flood envelope for the 200 year (0.5% annual average probability) plus a 20% allowance for climate change. Each Flood Risk Assessment shall include the agreed SEPA and Local Authority Flood Officer sensitivity analysis and methodology. Consultation with SEPA and Local Authority Flood Officer will be required in order to agree the scope prior to Flood Risk Assessment being carried out.

Sustainable drainage (SUDs) methods will be employed for the purposes of surface water management and SUDs features such as basins, ponds and infiltration features will be located above the 200 year plus climate change envelope in accordance with recognized procedures.

The permissible development surface water run-off flow rate has been set at 3.55 litres per second per hectare and this run off rate has been established and agreed as the base criteria for the area with Moray Council.

9.3: UTILITIES

9.3.1: EXISTING OVERHEAD POWER LINES

The site area contains both transmission (132kV) and distribution (11kV and 33kV) networks, predominantly on overhead steel lattice pylons and timber poles respectively.

The existing Primary Sub Station (PSS) located on Covesea Road provides a key installation within the SSE network for Elgin and SSE have confirmed that there is no intent to decommission or relocate the PSS.

Selected power lines may require diversion and certain overhead lines placed underground however this is normal development practice.

The primary sub station and the associated 11kV network will form the basis for the electrical supply to the masterplan development area.

9.3.2: EXISTING SUB STATION

The existing sub station on Covesea Road comprises a major installation within the SSE transmission and local power distribution network. SSE have advised that the installation will remain in place for the long term and that development activity therefore requires to take account of its presence.



EXISTING SUB STATION AND OVERHEAD POWER LINES



EXISTING SUB STATION AND OVERHEAD POWER LINES

9.3.3: OTHER UTILITIES

WATER SUPPLY

In order to establish the ability of the existing Scottish Water water supply network to provide potable water to a development area, a Pre Development Enquiry to Scottish Water is required to establish whether or not a Water Impact Assessment (WIA) is required. In the case of Findrassie it is almost certain that such an assessment will be required.

A development of the scale of Findrassie would usually suggest that a staged/ phased approach to WIA reporting is employed.

Scottish Water records indicate that a trunk water main runs through the Findrassie site, generally following the line of the Sey Burn and then following the 132kV pylon corridor line.

GAS SUPPLY

The north east side of Elgin is served by both Intermediate, medium and low pressure gas mains with the Intermediate pressure pipe following the line of the 132kV pylon line to the east of Lossiemouth Road.

TELECOM SUPPLY

Development infrastructure works (roads, utility servicing) conventionally include the installation of telecom duct work and chambers with materials supplied free of charge by BT Openreach.



10.0 : TRANSPORT

ROADS STANDARDS TABLE

PRIMARY STREET - the core spine road running the length of the site from the Duffus Road area to the Lossiemouth Road area. Designed to allow public transport access.

Design Speed	<30mph	TMC Designing Streets Interpretation Note
Width	6.0m	TMC Designing Streets Interpretation Note
Curve Radii	No minimum but localised widening to ensure two buses / or refuse vehicles can pass each other - to be confirmed with swept path analysis.	Designing Streets / TMC Designing Streets Interpretation Note
Junction radii at priority junctions	Where joining external road network - 8m Where joined by minor roads - quadrant kerbstones	Table 18.1 of Feb 15 Aberdeenshire Standards Designing Streets
Forward Visibility	Min 45m To be restricted through the use of building positions and landscaping	TMC Designing Streets Interpretation Note
Junction Visibility	With Lossiemouth Road - at signals. With Covesea Road - tbc dependent on final junction arrangement. With Duffus Road - 4.5 x 215. Where joined by minor roads - 2.4m x (range) m	Design Manual for Roads and Bridges / TMC Designing Streets Interpretation Note
Footway	2.0m wide on both sides of street [except where development is to one or neither side in which case, footway can be provided on single side only].	Table 15.4 of Feb 15 Aberdeenshire standards [and para 12.4]
Parking	Limited on street around mixed use centres to be accommodated by subtle widening within street or end on or angled parking within a square.	Designing Streets

ROADS STANDARDS TABLE

SECONDARY STREET - e.g. streets forming connecting loops and giving access within larger development blocks

Design Speed	<30mph	TMC Designing Streets Interpretation Note
Width	5.5m	
Curve Radii	No minimum but localised widening to ensure refuse truck can use - to be confirmed with swept path analysis. Quadrant kerbstones	Designing Streets / TMC Designing Streets Interpretation Note
Junction radii at priority junctions	Min 11m	Designing Streets
Forward Visibility	To be restricted through the use of building positions and landscaping.	Designing Streets
Junction Visibility	2.4m x (range)m	TMC Designing Streets Interpretation Note
Footway	2.0m wide on both sides of street [except where development is to one or neither side in which case, footway can be provided on single side only].	Local Development Standard
Parking	Limited on street to be accommodated by subtle widening within street or end on or angled parking within woodland edge. Trees and street furniture used to discourage indiscriminate parking.	Designing Streets

ROADS STANDARDS TABLE

LANES / SHARED SURFACES / COURTYARDS - providing access to housing units

Design Speed	<10mph	TMC Designing Streets Interpretation Note
Width	3.7 m minimum	TMC Designing Streets Interpretation Note
Curve Radii	No minimum	Designing Streets
Junction radii at priority junctions	Quadrant Kerbstones	Designing Streets
Forward Visibility	Min 11m To be restricted through the use of building positions and landscaping.	Designing Streets
Junction Visibility	Generally level surfaces but 2.0m wide footpaths where required.	Designing Streets
Footway	With minor roads - 2.4m x (range)m	TMC Designing Streets Interpretation Note
Parking	Spaces defined with trees and street furniture used to deter indiscriminate parking.	Designing Streets

10.1 :ROADS DESIGN STANDARDS

There is an opportunity to raise the profile of the streets in Findrassie by adopting the principles of Creating Places and other National Policies and Guidelines.

In addition to future planning applications there are a number of other consents that will inform the development layout such as Road Construction Consent (RCC). It is recognised that the Council's adopted guidance on road standards reflects that in Aberdeenshire except for specific variations on parking standards and rural roads. More recently the Council has moved away from this guidance to rely more heavily on the February 2014 National Roads Development Guide. Other design advice is contained within the national 'Designing Streets' policy document and in the Council's own interim guidance on Designing Streets.

Designing Streets makes it clear (page 3) that existing local authority guidance on street design should be revised to be consistent with Designing Streets although local guidance should also ensure that street design responds to local context.

In order to minimise the risk that imposition of engineering standards poses to masterplanned layouts a more integrated approach to approval is encouraged. This requires collaboration between applicants, planning officers, RCC engineers and other stakeholders. This ensures that roads approval staff can be satisfied with the fundamentals of a development proposal and can provide approval in principle prior to the granting of planning permission.

This section of the Guidance has been developed to set out a proposed set of standards for development roads within the Findrassie Masterplan area and is included as part of the Supplementary Guidance by The Moray Council both as a planning and roads authority.

Where design parameters are noted, a suitable reference is given for these standards so that the Council and the applicant(s) can proceed to use this table to progress planning and roads consents. **SEE ROADS STANDARD TABLES (LEFT)**. The table provided outlines the definition and principles of the 3 types of street within the new neighbourhood.

As per the guidance in Designing Streets, the design of the streets will accommodate service vehicles without allowing their requirements to dominate the layout. As low traffic flows and speeds are generally anticipated, vehicles will be able to use the full width of the carriageway to manoeuvre. At any location where it appears necessary, vehicle tracking can be undertaken to support the layout design. The swept path analysis will take account of slow vehicle design speeds which will allow vehicles to take tight radius turns over short distances. Road designs will keep vehicle tracking paths as tight as possible.

11.0 : RISK



11.1: HEALTH AND SAFETY



There are a number of health and safety aspects to consider when developing a masterplan for 1500 houses within an area close to existing services and amenities such as Myreside Loch and the Sey Burn. Each of these elements have been considered as part of the new neighbourhood and integrated into the new community to increase the sense of place and identity. The design has however taken cognisance of relevant health and safety guidance from HSE and ROSPA. This guidance will also inform future development and detailed planning applications. The proximity to the existing overhead power lines and sub-station has also been considered. The sub-station will be screened on three sides by woodland and the proposal to erect a screen wall along the Covesea Road elevation is being investigated with the local authority and Scottish Power. This is however out-with the boundary of the development of Findrassie. The overhead power lines are being relocated underground through the western part of the development area. Where this has not been possible a wayleave and associated open space is provided in the east. This is not dissimilar to the relationship of housing and overhead power lines in other parts of the country.



12.0 : SUMMARY



10.0: CONCLUSION / SUMMARY



The purpose of this supplementary guidance is to provide a flexible framework for the local authority and the landowner to ensure the vision for Findrassie is delivered throughout its implementation. It should also act as guidance for developers in creating a new neighbourhood which is unique to the north of Elgin and which is an attractive place where people want to live, work and play.

The guidance consists of three constituent parts which should be considered together to deliver the vision:

VOLUME 1. Vision / Masterplan

VOLUME 2. Design Code / Character Zones

VOLUME 3. Background Information

It should also be considered alongside the Landscape Elements document which is included within the appendices. These documents will ensure that the masterplan for Findrassie is usable, deliverable and can be implemented.


This is a consultative draft of the supplementary guidance which is expected to be modified throughout the formal consultation process.

13.0 : FUTURE APPLICATION CHECKLIST



11.0: FUTURE APPLICATION CHECKLIST

The expansion area will be developed over a number of years, possibly by different developers. The supplementary guidance and framework provides a robust three-dimensional urban design strategy to co-ordinate future planning applications for the site. Each planning application should however be accompanied by a design statement and supporting information identifying:

- 
- ☐ How the design(s) have been informed by the contextual analysis of the site and its surroundings, including how the design fits within the landscape framework and urban structure of development;
 - ☐ The proportion and location of building types within the character zones, including indicative house types, sizes and tenures, and the integration of affordable houses, as well as the provision, location and means of integrating housing with any other uses;
 - ☐ Design concepts for the main public spaces and thoroughfares of each of the character areas, including indicative building designs, heights, typical streetscapes, and the palette of materials and colours to be used in each;
 - ☐ Indicative building designs, colours and materials to be used on buildings to secure cohesive and harmonising development areas;
 - ☐ Three dimensional design concepts of the public spaces within the character areas, illustrating the sequence of public spaces and views created by the building layout and their visual relationship with proposed focal points;
 - ☐ Visualisations and descriptions of the form of the new local hub, primary school and employment areas;
 - ☐ How the design seeks to integrate with the existing road, footpath, cycle network and proposed movement strategy of the masterplan;
 - ☐ How the design seeks to minimise resource consumption in its response to the contextual analysis through layout and building design, while complementing other design requirements;
 - ☐ How the design makes provision for flood management and can accommodate appropriate mitigation in areas identified as being at risk of flooding;
 - ☐ The type, location and incorporation of Sustainable Urban Drainage Systems and any required provision for waste recycling facilities;
 - ☐ How the various phases accord with the overall consistency of design and layout, and the provision of shared infrastructure;
 - ☐ Sustainability Statement.

